

# CHAPTER 4 **Allowable Uses, Development Standards, and Guidelines**

## **4.1 PURPOSE**

### **4.1.1 Introduction**

This chapter provides detailed regulations for development and land uses within the Specific Plan area, and describes how these regulations will be used as part of the City's development review process. This chapter is intended to provide for the continuing transformation of the Gateway area and achieve the vision and objectives identified in Chapter 1 (Introduction), and implement the plans, policies, and guidelines identified in the other chapters of this Specific Plan.

### **4.1.2 Applicability**

Except as otherwise provided by this Specific Plan, no uses or structures shall be established, substituted, expanded, constructed, altered, moved, maintained or otherwise changed, and no lot lines may be created or changed, except in conformance with this chapter, as follows:

a. **Regulating Plan and Zones (Section 4.2)**

The Regulating Plan defines the zones within the Specific Plan, identifies the parcels included within each zone, and describes their purposes and land uses.

b. **Allowable Uses (Section 4.3)**

This section identifies the type of land uses allowed in each of the zones established by the Regulating Plan. A parcel within the Specific Plan area shall only be occupied by land uses as allowed within the applicable Zone identified on the Regulating Plan (Section 4.2) and subject to the applicable type of City approval (for example, Site Development Permit, Use Permit, and so on as identified in this section or the City's Municipal Code). This section also establishes the Development Capacities within the Specific Plan area (Section 4.3.3), Minimum and Maximum Densities (Section 4.3.4), and the Development Entitlement Management System (Section 4.3.5).

c. **Development Standards (Section 4.4)**

This section identified densities to be permitted for each Planning District and Land Use Designation and regulates the features of site development and buildings that contribute to the urban form and affect the public realm. The standards regulate building height, property setbacks, treatment of building elevations, open space, parking, on-site open space, building types, streetscape design, and signage.

d. **Design Guidelines (Section 4.5)**

This section defines guidelines contributing to urban form and character that complement the standards specified by Section 4.4. These address property setbacks, building elevations, parking, on-site open space, building types, and streetscape design.

e. **Nonconforming Uses, Lots, and Structures**

To effectively implement the provisions of this Specific Plan and to protect the public health, safety and welfare, land uses, lots, and structures that were lawfully established, but that do not conform to the provisions of this Specific Plan, shall be regulated consistent with Subarticle 12 of the Laguna Niguel Zoning Code, Non-Conforming Uses, Lots, and Structures (Laguna Niguel Municipal Code Sections 9-1-120 through 9-1-127).

### **4.1.3 Administration**

The standards and other requirements of this chapter shall be administered and enforced by the City of Laguna Niguel Community Development Department and other departments in the same manner as the provisions of the Laguna Niguel Municipal Code. Unless specified otherwise, the provisions of this Specific Plan take precedence over the applicable provisions of the Laguna Niguel Municipal Code.

Development and use of property within the Laguna Niguel Gateway Specific Plan area shall clearly demonstrate compliance with the applicable requirements of this Chapter 4 (Allowable Uses, Development Standards, and Guidelines). In addition, consistency with the objectives, policies, plans, and guidelines contained in other chapters of this Specific Plan must also be demonstrated.

When a development issue arises that is not covered under the provisions of the Specific Plan, the City of Laguna Niguel Municipal Code shall apply.

Should a development provision within this Specific Plan be inconsistent with any development provision found elsewhere in this Specific Plan, the Community Development Director shall determine which provision is applicable.

## **4.2 REGULATING PLAN AND ZONES**

This section establishes five unique zones that are applicable to the properties within the Specific Plan area and include:

- Retail Commercial (RC) Zone
- Business Park (BP) Zone
- Community Service (CS) Zone
- Mixed-Use (MU) Zone
- Open Space (OS) Zone

The Regulating Plan (Figure 4-1) defines the boundaries of these zones and assigns a specific zone to each individual property within the Specific Plan area. The location of the zones is based on the desired distribution and mix of uses, development densities, and urban form characteristics identified in Chapter 3 (Policies and Development Plans). The zones are intended to accommodate the development of multiple new mixed-use districts where the placement of buildings, form and scale, orientation to sidewalks and the public realm, location of parking, and architectural character promote the interaction among living, working, shopping, and entertainment functions and walkability.

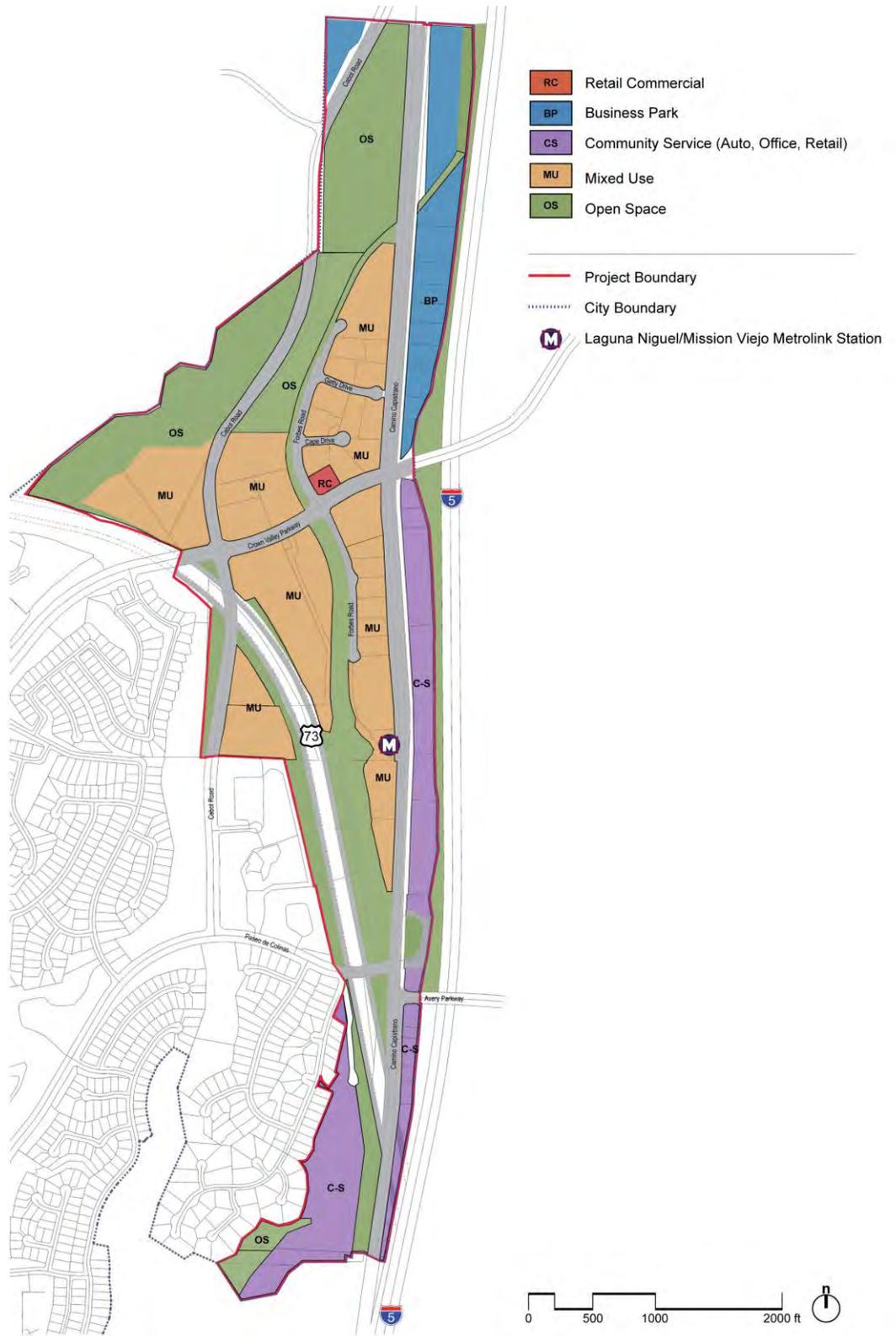


FIGURE 4-1 Regulating Plan

Each of the zones is defined as follows:

### **Retail Commercial (RC) Zone**

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The Retail Commercial (RC) Zone accommodates a diversity of commercial and personal service uses serving residents within and adjoining the Gateway area, those traveling on the freeways and major arterials, and workers in its offices and other businesses. Retail stores (supermarkets, furniture, appliances, etc.), general services (beauty stores and barbershops, copy shops, etc.), dining, and movie theaters are illustrative of the range of possible uses in this zone. Hotel and supporting uses such as restaurants, conference meeting room and banquet facilities may also be developed in this zone. Development densities range from Floor Area Ratios (FAR) of 0.35 to 0.5.



Community-serving retail uses with pedestrian walkways and plazas.

### **Business Park (BP) Zone**

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The Business Park (BP) Zone accommodates light manufacturing, warehousing, auto services, general services, restaurant, public utilities, and other similar uses. This includes many uses whose characteristics and operations require them to be separated from the other residential and mixed-use zones of the Specific Plan. Densities are limited to a FAR of 0.5.

### **Community Service (CS) Zone**

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The Community Service (CS) Zone accommodates a mix of light industrial, business park, office, commercial and auto service uses. This zone also provides for auto sales and services. Properties may also be used for surface or structured parking for the Metrolink Station. Densities are limited to a FAR of 0.5. Hotel and supporting uses such as restaurants, conference meeting room and banquet facilities may also be developed in this zone.

### **Mixed-Use (MU) Zone**

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The Mixed-Use (MU) Zone is intended to encourage development of an active urban environment that exhibits the character of distinct and a vibrant pedestrian friendly “village” and transit corridor where residents live, work, dine, are entertained, and recreate, with easy access to Metrolink transit. It allows for the intermixing of a diversity of land uses that will reduce vehicle trips and facilitate walking. Any property may be developed exclusively for office, multi-family, or

hotel uses, or mix of these with retail commercial integrated into one or more building vertically or distributed horizontally on a single site.

The mix of uses will be unified by their urban form and relationship to street frontages and adjoining parcels. While more than one use may be located on any block within the zone, all buildings shall be placed on their lot, oriented to the street frontage, and designed to convey an urban character.



Office building with retail shops located on the ground floor.

The development of office uses in the MU Zone is intended to provide employment opportunities for residents of Laguna Niguel and adjoining communities. Illustrative uses include medical offices, banking facilities, insurance sales, property management and leasing agencies, real estate sales, and professional offices for tenants such as architects, landscape architects, and software developers. Office uses must be developed in multi-story structures with FARs ranging between 0.5 and 1.0 with parking primarily located in structures, contributing to the intended urban character of the Specific Plan area. Densities for office development may be increased to a maximum FAR of 2.0 in exchange for the provision of extraordinary benefits for the greater Gateway community. Illustrative of these are the inclusion of community-serving meeting rooms and facilities, public parking exceeding project-related code requirements, and/or funding of non-project infrastructure and open space improvements.

The development of multi-family housing units in the MU Zone is intended to enable residents to live in proximity to their jobs, commercial services, and transit, thereby reducing automobile trips, commuting distances, and greenhouse gas emissions while improving their quality of life. These shall be located and designed to convey an “urban” scale and character, typical of those found in city centers and at transit nodes. They shall be constructed at densities between 40 and 50 dwelling units per acre, and up to 120 dwelling units per acre in exchange for the provision of important community benefits, as described for office uses, as well as affordable housing. Buildings may be constructed as mid-rise Class V structures. The units may be located on podiums above parking decks or wrap around parking structures. It is preferable that units be located along or no more than half a floor above the ground elevation. On-site resident-oriented recreational amenities and community facilities, such as meeting rooms, pools, spas, patios, may also be accommodated as ancillary uses in this zone.



40 units per acre



80 units per acre



120 units per acre

Mixed-use buildings may be developed in the MU Zone integrating office and/or retail uses with multi-family residential units. The housing units must be constructed above or to the rear of street-facing non-residential uses. Mixed-use projects may be built to a minimum FAR of 1.5 and maximum of 2.0, where there is no more than 0.7 FAR may be occupied by retail uses and 1.0 FAR for office use with the balance developed for housing. Their densities may be increased to a maximum FAR of 3.0 as an incentive for the provision of community benefits described for office and multi-family housing uses above.

Hotel and supporting uses such as restaurants, conference meeting room and banquet facilities may also be developed in this zone.

Where feasible, shared parking structures should be developed to reduce the need for multiple and inefficient driveways and parking lots and promote the continuity of the building wall along the street frontage. Such structures should be located below or behind buildings.



Mixed-use housing and retail; FAR 1.5



Mixed-use housing, office, and retail; FAR 3.0

## Open Space (OS) Zone

The Open Space (OS) Zone is intended to retain lands in a natural and undeveloped state due to their physical constraints of topography and natural habitat. They may be used for active and passive outdoor recreation and interpretative facilities, provided that any physical improvements maintain the integrity of the natural resources. These areas include hillsides and slopes, highway underpasses, Oso Creek drainage channel, and Galivan Basin.

## 4.3 ALLOWABLE USES

### 4.3.1 Establishment of an Allowable Use

A lot or building within the Specific Plan area shall only be occupied by the land uses identified in Table 4-1 (Allowable Land Uses and

Permitting Requirements, Laguna Niguel Gateway Zones) within the zone applied to the site by the Regulating Plan.

Any one or more land uses identified by Table 4-1 as being allowed in a specific zone may be established on any lot within that zone, subject to the applicable approval and processing requirements listed in the table, and consistency with the requirements of Subarticle 11 (Permits and Amendments) of the Laguna Niguel Zoning Code. In addition, all uses must be consistent with Section 4.3.3 (Development Capacities), Section 4.3.4 (Minimum and Maximum Densities), and Section 4.3.5 (Development Entitlement Management System).

- **P = Permitted Use**—The use is permitted as a principal use within the zone.
- **A = Accessory Use**—The use is permitted only if accessory to the principal use on the site.
- **U = Use Permit**—This use is permitted subject to approval of a use permit by the Planning Commission.
- **M = Minor Use Permit**—The use is permitted subject to approval of a minor use permit by the Community Development Director.
- **T = Temporary Use Permit**—This use is permitted only temporarily.
-  **Prohibited Use**—This use is not permitted within the zone under any circumstances. Consideration of a prohibited use may only be accomplished through approval of a Specific Plan Amendment by the City Council.

### 4.3.2 Land Uses Not Listed

A land use not listed in Table 4-1 is not allowed within the Specific Plan area. A land use that is listed in the table, but not in a particular zone, is not allowed in that zone.

Because not every possible use can be identified in this Specific Plan, and because new uses evolve over time, similar and compatible uses not listed may be allowed as either permitted outright, permitted with a use permit, or not permitted within a particular zone or Specific Plan area, consistent with Section 9-1-23 of the Laguna Niguel Municipal Code.

<b>TABLE 4-1 Allowable Land Uses and Permitting Requirements, Laguna Niguel Gateway Zones</b>					
KEY: P = Permitted Use    A = Accessory Use    U = Use Permit    M = Minor Use Permit T = Temporary Use Permit <span style="background-color: #cccccc; border: 1px solid black; display: inline-block; width: 1em; height: 1em;"></span> = Prohibited Use					
Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
<b>Residential, Lodging, and Group Care Facilities</b>					
Multi-family dwelling units, not as part of mixed-use project				P	
Multi-family dwelling units as part of mixed-use project with retail, restaurants, services, office, or similar uses				U	
Live/work units				U	
Home occupations, subject to §9-1-35.10 of the Laguna Niguel Municipal Code.				A	
Emergency shelters <sup>a</sup>		P	P		
Transitional and supportive housing		P	P	P	
Single room occupancy (SRO) hotels, subject to §9-1-45.25 of the Laguna Niguel Municipal Code.		U	U		
Hotels or motels			U	U	
Child day care facilities, centers, and preschools, subject to §9-1-45.24 of the Laguna Niguel Municipal Code				U	
Caretaker residences, subject to §9-1-45.15 of the Laguna Niguel Municipal Code		U			
<b>Retail</b>					
Retail stores, under 2,500-square-foot (sf) floor area	P	P	P	P	
Retail stores, 2,500 to 30,000 sf floor area	P	P	P	P	
Retail stores, 30,000 to 60,000 sf floor area	P	P	U	P	
Retail stores over 60,000 sf floor area	P	U	U	P	
Convenience stores <sup>b</sup> open less than 18 hours/day	P	U	U	P	
Convenience stores <sup>b</sup> open 18 or more hours/day	U		U	U	
Liquor stores <sup>c</sup> —with no consumption of alcohol on the premises	U	U	U	U	
Plant nurseries and garden stores, subject to § 9-1-45.11 (Outdoor Display and Storage) of the Laguna Niguel Municipal Code.		U	U		
Fuel Dispensing Facility Accessory to a Retail Store over 100,000 sf in floor area, if the property is consistent with required landscaping standards and anticipated public right-of-way dedication along Crown Valley Parkway				U	
Showroom—catalog stores, without substantial on-site inventory		P	P		

**TABLE 4-1 Allowable Land Uses and Permitting Requirements, Laguna Niguel Gateway Zones**

KEY: P = Permitted Use A = Accessory Use U = Use Permit M = Minor Use Permit  
 T = Temporary Use Permit [ ] = Prohibited Use

Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
<b>General Services</b>					
ATM—walkup	P	P	P	P	
Barber shops, beauty, nail and tanning salons and similar uses		M	M	M	
Miscellaneous services such as travel services, photo developing, videotape rentals, shoe repair, appliance repair, and similar uses		P	P	P	
Laundromats and dry cleaners—except central cleaning plants		P	P	P	
Printing, blueprinting and copy services		P	P	P	
Postal annex, private		P	P	P	
Pet grooming—without overnight boarding		P	P	P	
Telecommuting centers		P	P	P	
Tutorial services and learning centers		M	M	M	
<b>Office Uses and Health Services</b>					
Banks and savings and loans		P	P	P	
General and professional offices		P	P	P	
Employment agencies		P	P	P	
Medical and practitioner offices and Surgicenters <sup>d</sup> under 5,000 sf aggregate floor area in one building		P	P	M	
Medical and practitioner offices and Surgicenters <sup>d</sup> 5,000 sf or more aggregate floor area in one building		U	U	U	
Hospitals and convalescent hospitals		U	U	U	
Veterinary clinics/animal hospitals, without pet boarding		P	P	U	
Veterinary clinics/animal hospitals, with pet boarding (indoor only)		U	U	U	
<b>Dining, Drinking, and Entertainment Uses</b>					
Restaurants (Category I and II as described in §9-1-66 of the Laguna Niguel Municipal Code)	M	M	M	M	
Restaurants, fast food (with no drive-thru or outdoor seating)	M	M	M	M	
Restaurants, fast food (with drive-thru)			U		
Restaurants, fast food (with outdoor seating)	M	M	M	M	

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Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
Restaurants: retail food sales with ancillary seating	M	M	M	M	
Bars, taverns and cocktail lounges	U	U		U	
Dancing or live entertainment as a principal use	U	U		U	
Dancing or live entertainment as an accessory use	U	U	U	U	
Theaters, live or motion picture	U			U	
<b>Recreation Uses</b>					
Skating rinks, ice or roller		U	U	U	
Bowling alleys and pool or billiard halls as a principal use		U	U	U	
Pool or billiard tables as an accessory use (three tables or less)	A	A	A	A	
Game machine arcades as a principal use	U			U	
Game machines as an accessory use (five machines or less)	A	A	A	A	
Game machines as an accessory use (six machines or more)	M	M	M	M	
Tennis clubs, golf courses, and similar recreation uses, lighted or unlighted (see PR district regulation in Laguna Niguel Zoning Code, Subarticle 5 of the Laguna Niguel Municipal Code)				U	
Indoor or Outdoor Commercial recreation centers and uses including such facilities as miniature golf, go-karts, bumper boats, batting cages, kiddie rides, rock climbing, and similar attractions, lighted or unlighted		U		U	
Indoor or Outdoor Commercial roller hockey, skateboard, and stunt-bike facilities, lighted or unlighted		U		U	
Health clubs, martial arts studios, and dance studios, under 5,000 sf floor area	M	M	M	M	
Health clubs, martial arts studios, and dance studios, over 5,000 sf floor area	U	U	U	U	
Libraries and museums	P	P	P	P	
Cultural centers and performing arts centers				U	
Parks and open space	P	P	P	P	P
Bicycle, equestrian, and hiking trails	P	P	P	P	P
Indoor pistol or rifle ranges		U			

**TABLE 4-1 Allowable Land Uses and Permitting Requirements, Laguna Niguel Gateway Zones**

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Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
<b>Assembly Uses</b>					
Lodges, union halls, social clubs, and senior citizen centers		U		U	
Churches, temples, and other places of worship		U	U		
Mortuaries and funeral homes		U	U		
<b>Public and Semipublic Uses</b>					
Fire stations	P	P	P	P	
Government offices and police stations/substations	P	P	P	P	
United States post office		P	P	P	
Public utility facilities	P	P	P	P	
Public flood control facilities and devices	P	P	P	P	P
Colleges and universities, including primary and satellite campuses and classrooms		U	U	U	
Vocational schools (e.g., barber, beauty, and similar)		U	U	U	
Metrolink, train, bus and taxi stations			P	P	
Helicopter pads as accessory use only				U	
Public or private kennels and animal shelters (with indoor pet boarding)		U			
<b>Automotive Uses</b> (subject to §9-1-45.11 [Outdoor Display and Storage] of Title 9 in the Laguna Niguel Municipal Code)					
Gas and service stations		U	U		
Car washes		U	U		
Auto body repair and painting; major engine and transmission repair		U	U		
Auto repair specialty shops as a primary use: Providing minor auto maintenance: tire sales/service, muffler, brake, lube, and tune-up services— not including major engine or drive-train repair		U	U		
Auto repair specialty shops same as above, but as an accessory use, subordinate to a primary use such as a discount store, warehouse store, or other permitted primary use		U	U	U	
Auto and motorcycle sales and rentals; new or used		U	U		
Truck, recreation vehicle, and boat sales		U	U		
Truck and/or equipment rentals		U	U		

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Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
Auto part stores (retail only, no repair)	P	P	P		
Auto or truck storage yards, not including dismantling		U	U		
Parking lots/garages as a principal use, subject to Subarticle 6 (Parking) of Title 9 in the Laguna Niguel Municipal Code		U	U	U	
Park-and-ride lots, subject to Subarticle 6 (Parking) of Title 9 in the Laguna Niguel Municipal Code		U	U	U	
<b>Warehousing and Heavy Commercial Uses</b> (subject to §9-1-45.11 [Outdoor Display and Storage] of Title 9 in the Laguna Niguel Municipal Code)					
Wholesaling/distribution centers, with no sales to consumers		P			
General warehouses, with no sales to consumers		P			
Mini-storage warehouses		U	U		
Lumber yards, outdoor (see retail stores for indoor lumber sales)		U	U		
Pest control services		P	P		
Plumbing repair shops		P	P		
Contractor, public utility, and similar equipment/storage yards		U	U		
Central cleaning or laundry plants		U	U		
Communication or relay facilities/antennas as a principal use, subject to §9-1-45.6 (Satellite Dish and Other Antennas) of Title 9 in the Laguna Niguel Municipal Code. Such facilities shall be considered a principal use if not associated with and incidental to another principal use on the premises.	U	U	U	U	U
<b>Industrial and Research Uses</b>					
Manufacture and assembly of components or finished products from materials such as cloth, fiber, fur, glass, leather, stone, paper (except milling), plastics, metal and wood		P	P		
Research and development		P	P		
Recording studios		P	P		
Welding, machine, and metal plating shops		U			
Recycling centers as a principal use, collection and sorting only, subject to §9-1-45.18 (Vending Machines and Recycling Facilities) of Title 9 in the Laguna Niguel Municipal Code.		U			

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Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
Off-site hazardous waste facilities, subject to §9-1-45.22 (Hazardous Wastes and Materials) of Title 9 in the Laguna Niguel Municipal Code.		U			
<b>Accessory Uses and Structures</b>					
Game machines as an accessory use	Refer to Recreation Uses				
Outdoor vending (such as flower stands, hotdog stands, etc.), subject to §9-1-45.10 of Title 9 in the Laguna Niguel Municipal Code.	M			M	
Swimming pools and spas as an accessory use				A	
Signs, subject to this Specific Plan and Subarticle 7 (Signs) of Title 9 in the Laguna Niguel Municipal Code.	A	A	A	A	A
Fences and walls, subject to §9-1-35.2 of Title 9 in the Laguna Niguel Municipal Code.	A	A	A	A	A
Antennas and satellite dishes, subject to §9-1-45.6 of Title 9 in the Laguna Niguel Municipal Code.	A	A	A	A	A
Reverse vending machines, subject to §9-1-45.18 (Vending Machines and Recycling Facilities) of Title 9 in the Laguna Niguel Municipal Code.		M	M		
Recycling drop-off bins, subject to §9-1-45.18 (Vending Machines and Recycling Facilities) of Title 9 in the Laguna Niguel Municipal Code.		M	M		
Incidental products or services for employees or businesses, such as child day care, cafeterias, and business support uses	A	A	A	A	A
Other accessory uses and structures that are customarily associated with and subordinate to the principal use on the premises and are consistent with the purpose and intent of the zoning district	A	A	A	A	A
<b>Temporary Uses</b>					
Christmas tree sales, subject to §9-1-45.7 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	
Halloween pumpkin sales, subject to §9-1-45.8 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	
Stands selling fresh produce in season, subject to §9-1-45.9 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	
Use of relocatable building, subject to §9-1-45.17 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	T

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Land Use Type	Permit Required by Zone				
	RC	BP	CS	MU	OS
Construction trailers and guard offices, subject to §9-1-45.16 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	T
Special outdoor events, subject to §9-1-45.13 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	T
Sidewalk sales, subject to §9-1-45.12 of Title 9 in the Laguna Niguel Municipal Code.	T	T	T	T	[ ]
<b>Other Uses</b>					
Fortune telling and palmistry	[ ]	U	U	[ ]	[ ]
Medical marijuana dispensaries	[ ]	[ ]	[ ]	[ ]	[ ]
Any use prohibited by Federal and/or State law	[ ]	[ ]	[ ]	[ ]	[ ]
Tattoo and permanent makeup services	[ ]	[ ]	[ ]	[ ]	[ ]
Adult businesses, subject to Subarticle 5 (Special Purpose Regulations) of Title 9 in the Laguna Niguel Municipal Code. <sup>e</sup>	[ ]	U	U	[ ]	[ ]
Other principal, accessory or temporary use not listed above	Director or Planning Commission to determine whether use is permitted in accordance with §9-1-23 of the Laguna Niguel Municipal Code.				
a. To be allowed by right with construction of or alteration to any structure subject to a minor site development permit in accordance with Zoning Code Section 9-1-114.1. b. Defined as retail stores under 10,000 sf selling primarily food, beer, wine, and beverage items for off-site consumption, but not distilled spirits c. Defined as retail stores selling primarily food, beer, wine, distilled spirits, and other beverages, plus some food items, all for off-site consumption d. Offices for physicians, dentists, optometrists, chiropractors, physical therapists, and similar practitioners e. Property must also be within AB (Adult Business) overlay district per §9-1-54.4 of the Laguna Niguel Municipal Code					

### 4.3.3 Development Capacities

This section establishes limits for the total amount of land use development in the Specific Plan area. The General Plan Land Use Element and Statistical Summary for Community Profile Area 4, which comprises the Gateway area, allows for a maximum of 2,994 dwelling units and up to 2,259,931 square feet of non-residential uses. Table 4-2 (Land Use Development Capacity) identifies the maximum type of development by Planning District (Figure 4-2). To accommodate greater flexibility within the Mixed-Use zones, the exact amount of residential units and non-residential square footage may be exchanged between Districts C, D, E, and H provided the overall development capacity of the Specific Plan area is not exceeded. Any such exchanges shall be considered as part of the discretionary application process for the specific development project and without the need for a General Plan or Specific Plan Amendment, if the decision-making authority finds that such exchange is:

- a. Consistent with all provisions of the Gateway Specific Plan
- b. Consistent with the established Development Entitlement Management System (DEMS, Section 4.3.4)
- c. Any additional traffic impacts that may be identified are mitigated

**TABLE 4-2 Land Use Development Capacity**

<i>Planning District</i>	<i>Residential (dwelling units)</i>	<i>Retail Commercial (square feet)</i>	<i>Office (square feet)</i>	<i>Business Park (square feet)</i>	<i>Hotel (rooms)</i>	<i>Automobile Sales (square feet)</i>
<b>A</b>	0	0	0	76,480		
<b>B</b>	0	0	0	323,200		
<b>C</b>	220	0	305,460	0	*	
<b>D</b>	200	0	187,639	0	200*	
<b>E</b>	1,427	87,338	203,425	0	*	
<b>F</b>	142	0	173,900	0		
<b>G</b>	142	247,639	0	0	*	
<b>H</b>	863	76,000	240,100	0	*/***	
<b>I</b>	0	62,509	30,492	0	***	45,739****
<b>J</b>	0	0	0	0		141,860****
<b>K</b>	0	58,150	0	0	150**	
<b>Totals</b>	<b>2,994</b>	<b>531,636</b>	<b>1,141,016</b>	<b>399,680</b>	<b>350</b>	<b>187,599</b>

\* A hotel with a maximum of 200 rooms may be located in planning districts C, D, E, G, or H.  
 \*\* A total of 150 motel/hotel rooms may be located within planning district K.  
 \*\*\* Up to a total of 1,200 parking spaces to serve the Metrolink station may be provided in areas H & I  
 \*\*\*\* Auto uses to occupy a total of 3.5 acres in District I and 14.3 acres in District J



### 4.3.4 Development Entitlement Management System (DEMS)

This section establishes the limits on the general issuance of permits and parameters under which an exchange of land uses may be permitted within Planning Districts C, D, E, and H as identified in Section 4.3.3 (Development Capacities). To provide flexibility to accommodate a wide range and combination of potential projects with retail, office, and residential components, the Development Entitlement Management System (DEMS) has been created to relate the amount of development to the amount of traffic that is generated within these critical areas of the Specific Plan. Table 4-3 (Automobile Trip Generation Capacity) establishes the thresholds for traffic generation that must be adhered to. The traffic generation thresholds are expressed in terms of the cumulative number of inbound and outbound trips in the AM and PM peak driving hours by the applicable Planning District.

These represent the cumulative trips that shall be allowed, including existing and new development. The limits shall be administered by the Department of Community Development as part of the development review process for individual projects, in concert with existing and remaining development capacity and existing and remaining trip generation capacity for each Planning District.

<i>District</i>		<i>AM Peak</i>		<i>PM Peak</i>	
		<i>Inbound</i>	<i>Outbound</i>	<i>Inbound</i>	<i>Outbound</i>
C & D	Existing 2010	95	50	60	75
	Net Additional	900	400	600	1,200
	<b>Total Future</b>	<b>995</b>	<b>450</b>	<b>660</b>	<b>1,275</b>
E	Existing 2010	175	105	200	250
	Net Additional	345	680	760	570
	<b>Total Future</b>	<b>520</b>	<b>785</b>	<b>960</b>	<b>820</b>
H	Existing 2010	175	100	75	180
	Net Additional	550	520	740	755
	<b>Total Future</b>	<b>725</b>	<b>620</b>	<b>815</b>	<b>935</b>

\* Trip capacities shall not apply to other planning districts.

No development project shall be considered that exceeds these trip capacity limits. Where a proposed land use exceeds the capacity for a category of use in a planning district and there is remaining

automobile trip capacity, the use may be considered by the decision-making authority as part of the discretionary application process, provided that it does not exceed the automobile trip capacity for that planning district.

When the total automobile trip generation capacity is reached, no new development shall be permitted in the planning district unless one or more of the following conditions is met:

- a. Additional mitigation is implemented that reduces traffic impacts on Crown Valley Parkway, Forbes Road, Cabot Road, and the Crown Valley Parkway/I-5 interchange to levels below those projected by the City's traffic model for the Gateway Specific Plan, where the land use and trip generation capacities specified in Table 4-2 and Table 4-3 may be adjusted to achieve equivalent levels of impacts.
- b. Traffic analyses are conducted that indicate actual land use trip generation in a planning district is less than calculated for development projects, where the net difference in trips can be allocated toward increased development capacity.
- c. Traffic analyses are conducted that indicate traffic volumes on Crown Valley Parkway, Forbes Road, Cabot Road, and the Crown Valley Parkway/I-5 interchange, attributable to regional trips, are below those assumed in the Gateway Specific Plan traffic model, whereas the difference may be considered as the basis for increases in development and trip generation capacity in the planning district.

Changes in total land use and trip generation limits shall be reviewed with the Planning Commission and approved by the City Council as an amendment to the Specific Plan.

The Department of Community Development in collaboration with the Department of Public Works shall maintain tables of current data regarding existing land uses, AM and PM peak hour trip generation, and remaining land use and trip generation capacities for each planning district. These shall be used as the basis for evaluation of proposed development applications. As proposed in conjunction with development applications, and at least once each five years, the City shall review traffic conditions on Crown Valley Parkway, Forbes Road, Cabot Road, the Crown Valley Parkway-I-5 interchange, and any other location deemed of relevance by the City to the conditions in the Specific Plan area, and determine the appropriateness of adjusting the land use and trip generation capacities. Modifications to the land use and trip generation limits based on these studies

shall be reviewed with the Planning Commission and approved by the City Council as an amendment to the Specific Plan by the City Council.

### **4.3.5 Minimum and Maximum Densities**

Densities that may be permitted for development within each Planning District and zone established by the Regulating Plan are specified in Section 4.4, Table 4-4 (Development Standards). All projects shall be limited to the baseline densities, and where indicated, conform to the minimum density requirement. Specific projects may be permitted to exceed the baseline densities, up to the stated maximum density for projects that make an extraordinary contribution in meeting the land use and urban form objectives for the Specific Plan. Representative “benefits” that may be considered for incentive bonuses include affordable housing, meeting rooms and recreational facilities accessible to the general public, architectural design and site development exceeding the City’s standards for environmental sustainability, and funding of streetscape and public realm improvements and amenities unrelated to those required for the project. The amount of increased density shall be determined by the decision-making body as part of the discretionary application process.

## **4.4 DEVELOPMENT STANDARDS**

### **APPLICABILITY**

All areas within the Specific Plan boundaries are subject to the City of Laguna Niguel Municipal Code, including Title 9 (Planning and Zoning), Division 1 (Zoning Code). In addition, applicability of the standards in this section is specified by both planning district (Figure 4-2) and land use designation. Table 4-4 (Development Standards Matrix) illustrates a summary of development standard requirements throughout the Specific Plan area. Sections 4.4.1 through 4.4.7 consist of additional standards that apply to specific conditions and are referenced in Table 4-4.

Whenever there is a conflict between the development standards set forth herein and the Laguna Niguel Municipal Code, the standards of the Laguna Niguel Gateway Specific Plan shall apply.

TABLE 4-4 Development Standards										
Development Standards			Planning District →		A	B	C, D, F	E, G, H	I, J, K	
			Land Use Designation →		BP	BP	MU	MU	RC	CS
<b>Lot Size</b>	Minimum Gross Lot Size (ac)				1	1	2	2	1	
<b>Non-residential Density</b>	<b>Retail</b>	FAR = Project Area / Gross Lot Area	Maximum, Project < 2 ac		0.35	0.35	0.35	0.35	0.35	0.35
			Maximum, Project > 2 ac		0.5	0.5	0.5	0.5	0.5	0.35
	<b>Office</b>		Maximum, Project < 2 ac		0.35	0.35	0.5	0.5		0.35
			Maximum, Project > 2 ac		0.5	0.5	1.0	1.0		0.35
			Maximum, Project > 2 ac with FAR Bonus				2.0	2.0		
	<b>Business Park</b>		Maximum, Project < 2 ac		0.6	0.35				
			Maximum, Project > 2 ac		0.6	0.5				
			Maximum, Project > 2 ac with FAR Bonus							
<b>Mixed-Use Buildings Density</b>		Total Building FAR	Minimum				1.5	1.5		
			Maximum, Project < 2 ac				1.5	1.5		
			Maximum, Project > 2 ac				2.0	2.0		
			Maximum, Project > 2 ac with FAR Bonus				3.0	3.0		
	<b>Housing/ Retail Mix</b>	Retail Portion	Minimum				0.35	0.35		
			Maximum, Project < 2 ac				0.5	0.5		
			Maximum, Project > 2 ac				0.7	0.7		
		Residential Portion	Minimum				1.15	1.15		
			Maximum, Project				1.65	1.65		
			Maximum, Project with FAR Bonus				2.65	2.65		
	<b>Housing/ Office Mix</b>	Office Portion	Minimum				0.35	0.35		
			Maximum, Project < 2 ac				0.7	0.7		
			Maximum, Project > 2 ac				1.0	1.0		
		Residential Portion	Minimum				1.15	1.15		
			Maximum, Project				1.65	1.65		
Maximum, Project with FAR Bonus					2.65	2.65				

TABLE 4-4 Development Standards								
Development Standards		Planning District →	A	B	C, D, F	E, G, H		I, J, K
		Land Use Designation →	BP	BP	MU	MU	RC	CS
Residential Density	Units per Acre	Minimum			40	40		
		Maximum			50	50		
		Maximum, with Density Bonus			120	120		
Building Height	Feet	Maximum	60		80 <sup>b</sup>	120	120	50 <sup>a</sup>
Setbacks	Perimeter Setbacks and Build-to Lines	From Street Right-of-Way	15 <sup>b</sup>		10	Per Section 4.4.1		10 <sup>b</sup>
		From Interior Property Line	5		5			5
		Nonresidential Abutting Residential or Mixed Use			60			60
		From Open Space District	10		10			10
		From Interior Property Lines within Same Project	0		0			0
Front Elevation	Height and Frontage				Per Section 4.4.2			
Parking	Required # of Spaces		Per Municipal Code Section 9-1-60 through 9-1-66					
	Parking, Drop-off Zones, Vehicular Circulation and Access Standards and Regulations		Per Municipal Code Section 9-1-60 through 9-1-66		Per Section 4.4.3		Per Municipal Code Section 9-1-60 through 9-1-66	
On-Site Open Space	Landscaping and On-Site Open Space	Minimum Interior Landscape (% of lot area)	10 <sup>c</sup>					10 <sup>c</sup>
		Publicly Accessible On-Site Open Space (% of lot area) (Per Section 4.4.4)			7	7	7	
		Open Space Street Frontage (ft.)		20 + (per Section 4.4.4)				
Building Types	Allowed per Land Use	Courtyard Podium			P	P		
		Urban Block			P	P	P	
		Liner Block			P	P	P	
		Wrapped Block			P	P		
		Per Section 4.4.5						
Streetscape Standards	Applicability by District			Per Section 4.4.6				

TABLE 4-4 Development Standards									
Development Standards		Planning District →	A	B	C, D, F	E, G, H		I, J, K	
		Land Use Designation →	BP	BP	MU	MU	RC	CS	
<b>Signage</b>	To Be Organized by Land Use							Per Section 4.4.7	
<p>P = permitted</p> <p>a. For the CS District on Camino Capistrano, south of Paseo De Colinas, building heights up to 80' are permitted subject to a Use Permit and Site Development Permit</p> <p>b. All minimum setbacks shall be increased one foot for each additional 5 ft (or portion thereof) that the building exceeds 35 ft in height; provided however, that the maximum setback required need not exceed 25 ft</p> <p>c. Represents the minimum interior project landscaping as a percentage of the net usable area of the project site. At least half of the required landscaping shall be located in areas devoted to parking. Required frontage and boundary landscaping, as well as paved plazas, courtyards, and other outdoor pedestrian gathering areas, may count toward this interior landscaping guideline. Retail centers of 3 acres or more within C/LI districts must provide minimum interior project landscaping of 15%</p>									

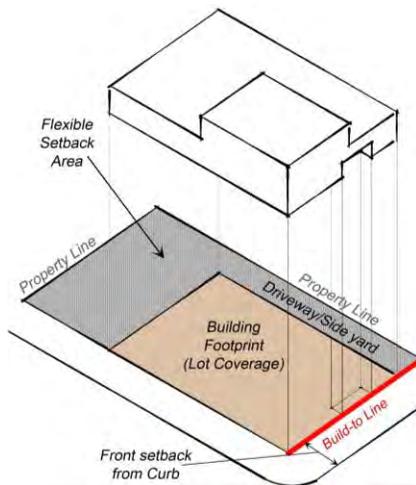
### 4.4.1 Setbacks

Setback standards and their applicability are included in Table 4-4. This section includes setback standards referenced in Table 4-4 for specific Planning Districts where higher pedestrian activity is expected to occur. Refer to Section 4.5.1 (Setbacks) for setback area design guidelines.

The setback and standards are based upon the recognition that compatibility of new buildings and additions depends in part on continuity of a street front setback and resulting lot coverage characteristics. 100% lot coverage is permitted where setback requirements allow it. The following two setback types apply to specific Planning Districts (Figure 4-2) as specified in Table 4-4:

#### A. URBAN CORE SETBACKS

- **Front setbacks:** Setback areas that are directly adjacent to any build-to lines, per Figure 4-3 (Build-to Line Setbacks/Parking Access), shall be treated as front setbacks and shall comply with the following requirements:



Setbacks and Lot Coverage

- > Setbacks shall be measured from future curb lines (after all infrastructure improvements) when adjacent to a street right-of-way and from property lines when not adjacent to a street right-of-way.
- > **East side of Forbes Road:** Setbacks along the east side of Forbes Road shall be measured from the future curb line and may vary from 18 to 38 feet as illustrated in Figure 4-4 (Forbes Road Setbacks, East Side, North of Crown Valley) and Figure 4-5 (Forbes Road Setbacks, East Side, South of Crown Valley).
- > **Getty Drive and Cape Drive:** A 20-foot minimum setback shall be measured from the curb as illustrated in Figure 4-6 (Getty Drive and Cape Drive Setbacks).
- > **Crown Valley Parkway:** An 18-foot minimum setback shall be measured from the curb as illustrated in Figure 4-7 (Crown Valley Parkway Setbacks).
- > Setback areas along build-to lines adjacent to the Laguna Niguel/Mission Viejo Metrolink Station, per Figure 4-3, shall be measured from the property line and be zero (0) feet.
- > Setback requirements along build-to lines do not apply when publicly accessible open space areas are provided along the front façade of a building. Refer to Section 4.4.4 (On-Site Open Space).

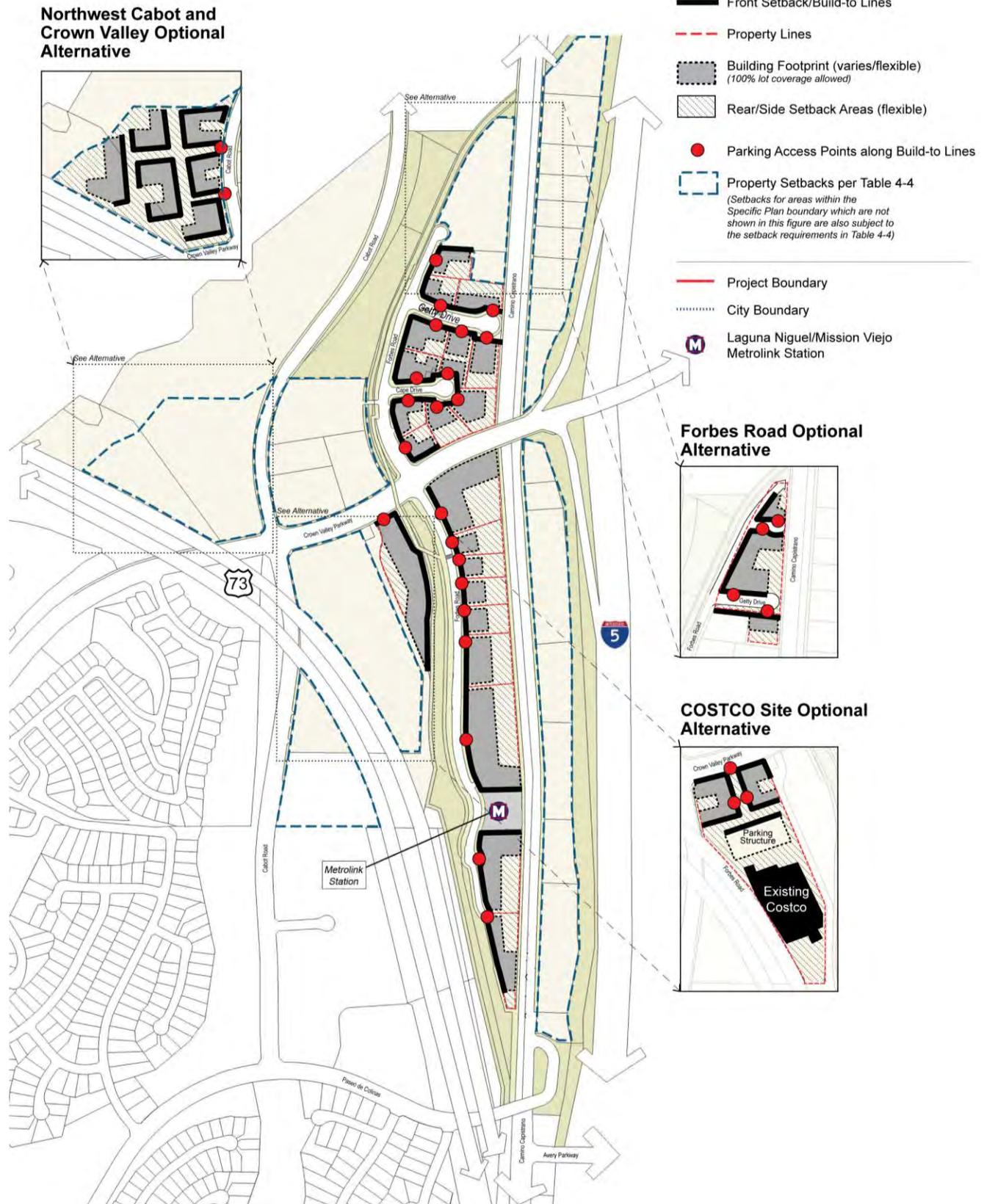


FIGURE 4-3 Build-to Line Setbacks/Parking Access

Existing - Typical

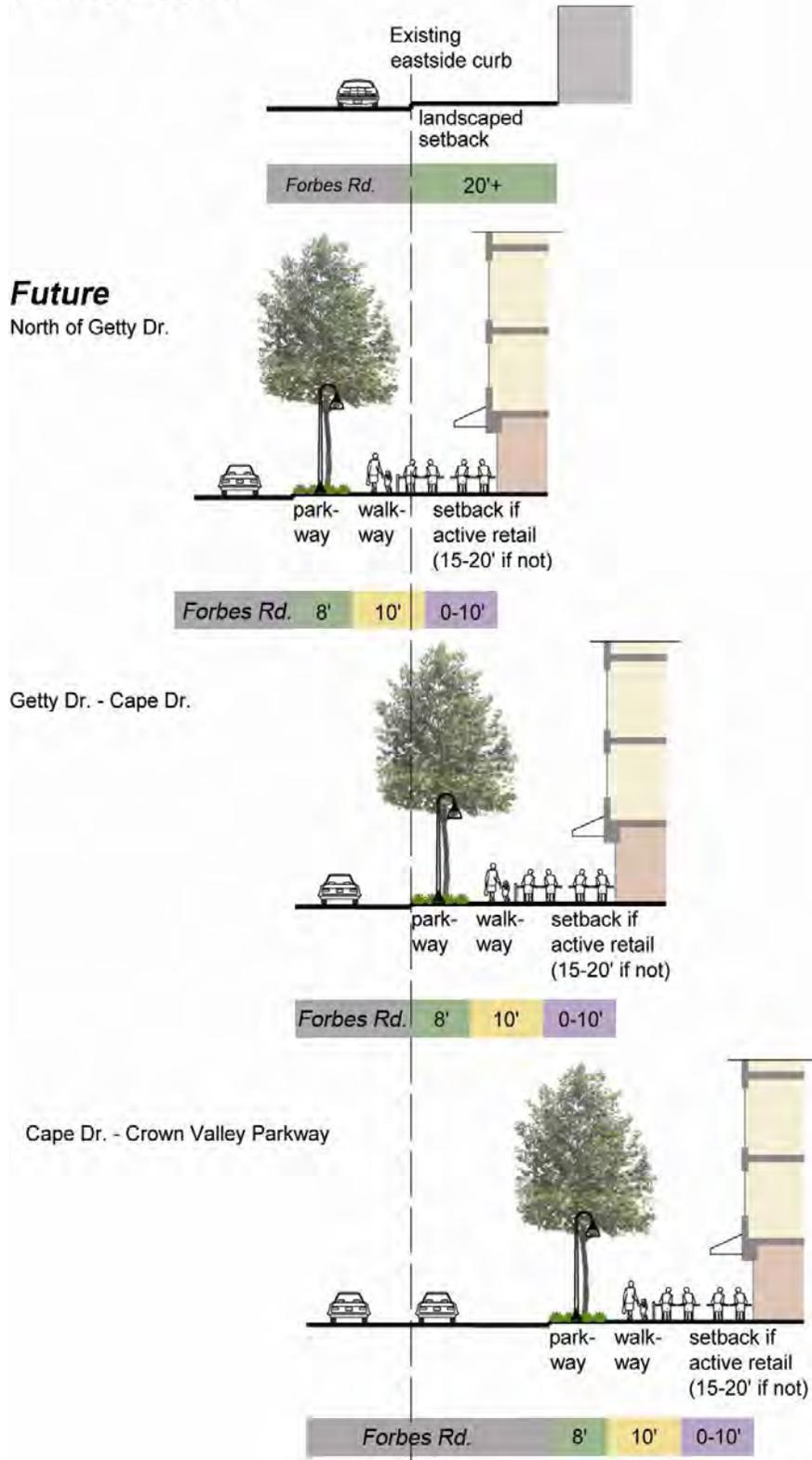
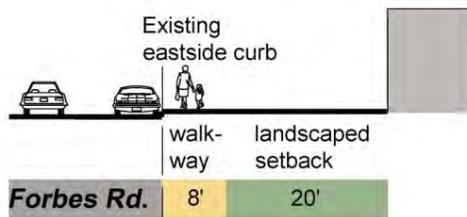


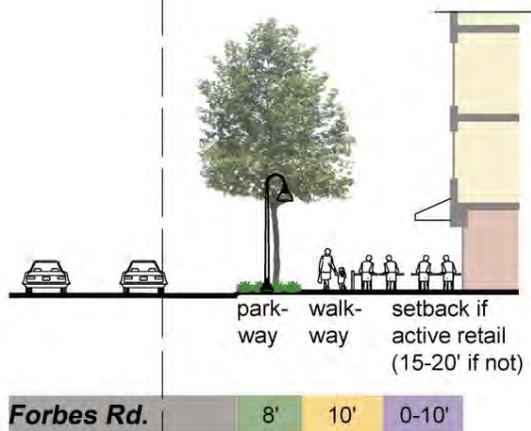
FIGURE 4-4 Forbes Road Setbacks, East Side, North of Crown Valley

Existing - Typical



Future

Crown Valley Parkway - 225' south



225' south of Crown Valley Parkway - end

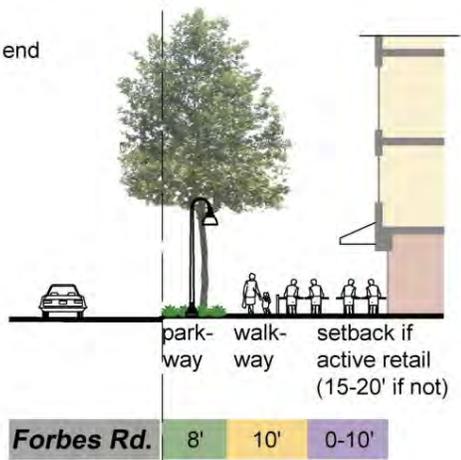
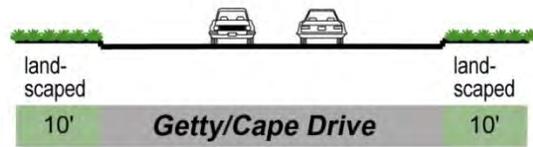


FIGURE 4-5 Forbes Road Setbacks, East Side, South of Crown Valley

Existing - Typical



Future

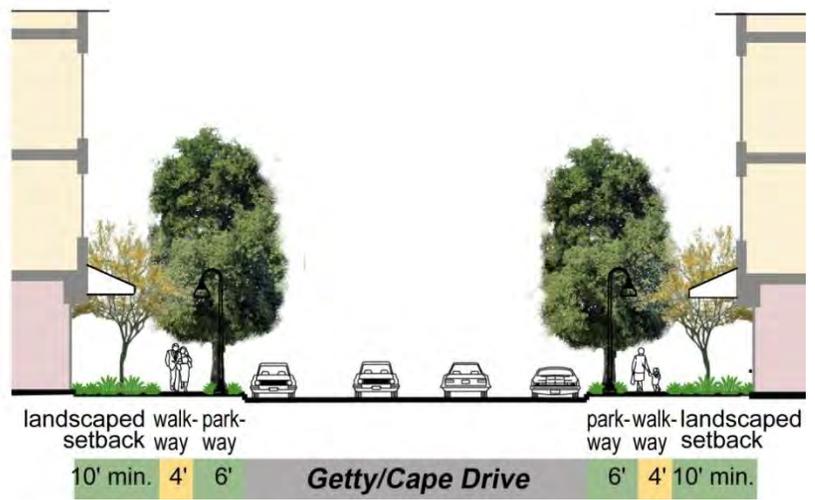
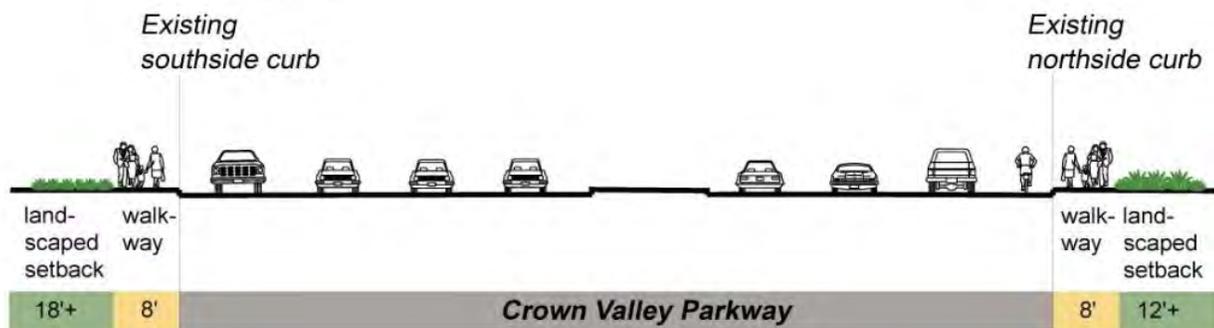


FIGURE 4-6 Getty Drive and Cape Drive Setbacks

**Existing - Typical**



**Future**

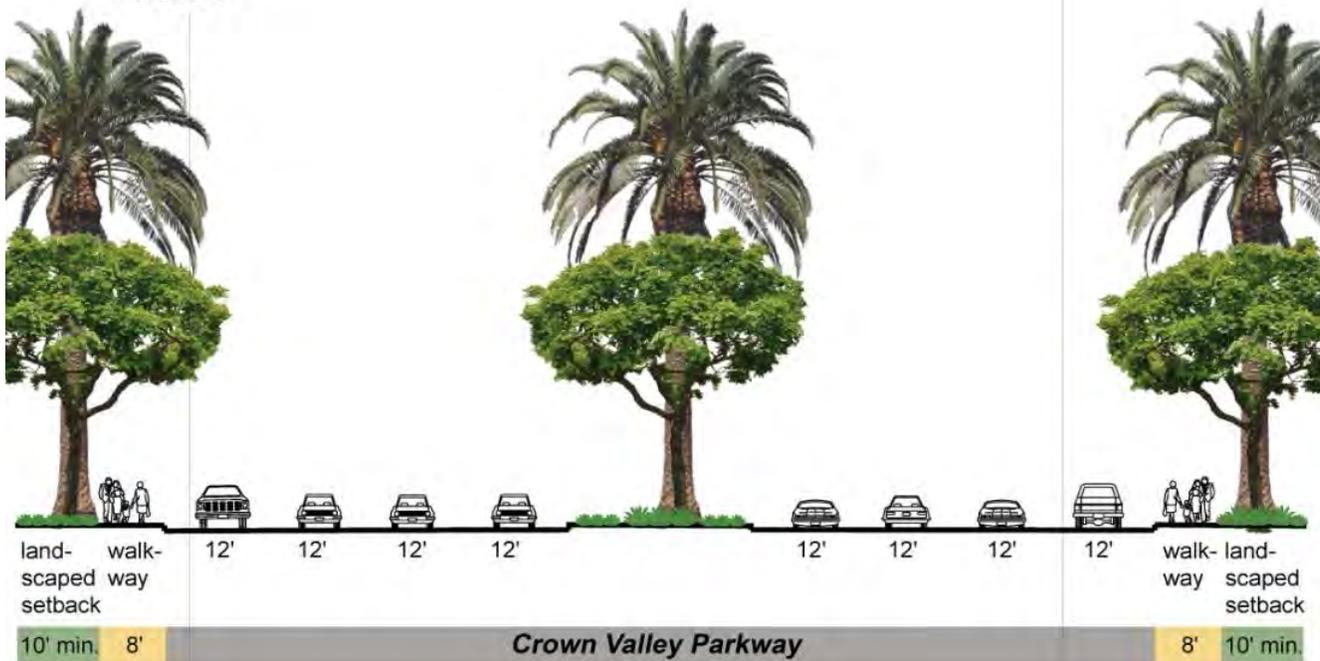


FIGURE 4-7 Crown Valley Parkway Setbacks

- **Rear and side setbacks:** Setback areas that are not directly adjacent to any build-to lines, per Figure 4-3, shall be treated as rear and side setbacks and shall comply with the following requirements:
  - > Setbacks shall be measured from future curb lines (after all infrastructure improvements) when adjacent to a street right-of-way and from property lines when not adjacent to a street right-of-way.
  - > **Crown Valley Parkway:** 18-foot minimum setback (Figure 4-7) measured from the curb.
  - > All other rear and side setback areas, per Figure 4-3, shall be flexible, with no minimum or maximum required.

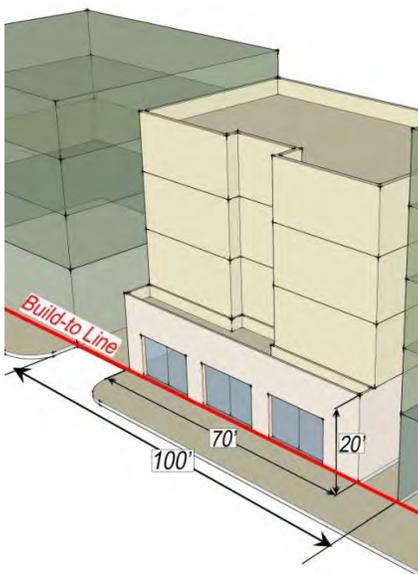
## B. INTERNAL PRIVATE-STREET DEVELOPMENT SETBACKS

- Buildings shall be located to front directly onto and be oriented to roadways, sidewalks, and/or public plazas developed internally within a large property or multiple adjacent properties as illustrated for the Costco site and northwest Cabot and Crown Valley Parkway site, in Figure 4-3, as optional development alternatives. Building frontage setbacks for any new internal streets are illustrated in Figure 4-8.

### 4.4.2 Building Elevations

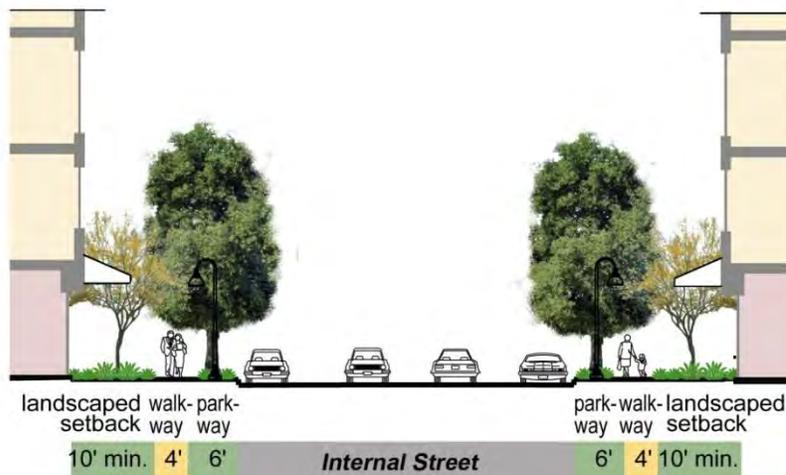
The building elevation standards in this section shall apply as specified in Table 4-4 (Development Standards). Refer to Section 4.5.2 (Building Elevations) for building elevation design guidelines.

- Where a build-to line front setback is required, per Figure 4-3, a minimum of a 20-foot-high building elevation shall be maintained along a minimum of 70% of the length of the property line adjoining the public right-of-way.



20-foot-high street-facing building elevation along 70% of the length of the property line adjoining the public right-of-way

**Future**



**FIGURE 4-8 New Internal Street Setbacks**

**4.4.3 Parking**

Parking configuration, placement, and access will be essential to the function and vitality of the Specific Plan area. Parking areas which are visible from main street fronts lead to a loss of vitality and street life.

The parking standards and requirements in the Laguna Niguel Municipal Code (Sections 9-1-60 through 9-1-66) shall apply, except as modified or supplemented by this Section, as specified in Table 4-4. Refer to Section 4.5.3 (Parking) for parking design guidelines.

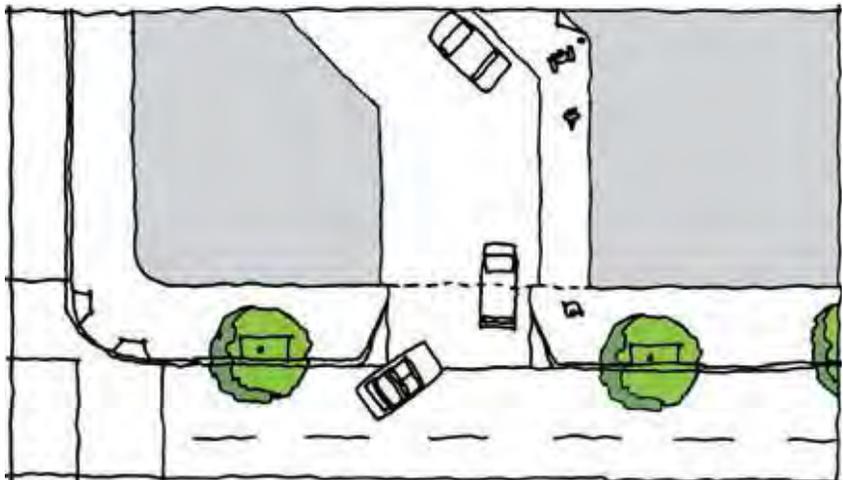
■ **Placement, Vehicular Circulation, and Access**

- > Semi-subterranean parking in mixed-use or non-residential buildings shall not extend to front street property lines but may extend to side or rear property lines provided it is completely screened from street view by landscape treatments and the aboveground section of the semi-subterranean parking garage does not rise up more than 4 feet from ground level.
- > Except for the ground-level frontage required for access to parking, no parking or loading shall be visible on the ground floor of any building façade along the build-to line, per Figure 4-3.

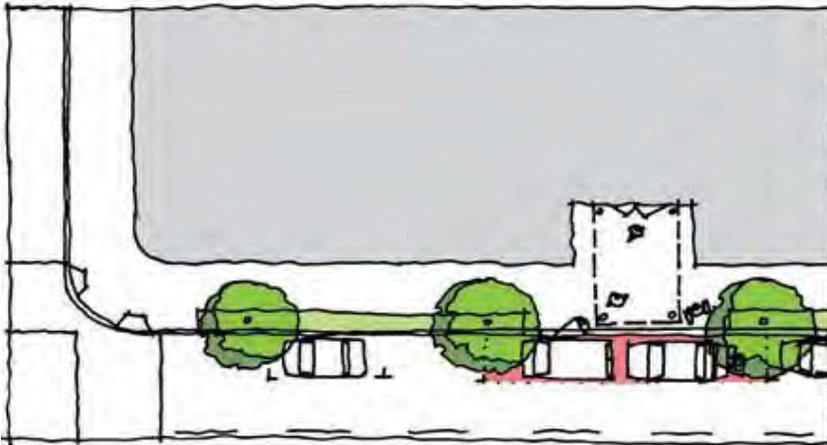
- > Curb cuts and parking/loading entries into buildings shall be limited to the minimum number and width required by the City's Municipal Code.
- > Parking and loading access shall be shared when feasible.
- > Parking and loading access shall be located a minimum of 25 feet from a primary building entrance, pedestrian paseo, or public outdoor gathering area. This does not apply to a hotel or residential porte cochere.
- > Drive-through aisles for fast food or similar use shall not be permitted along Forbes Road.
- > Service, trash enclosures, and loading facilities shall be blocked from view from public streets and public open space areas.

■ **Drop-Off Zones**

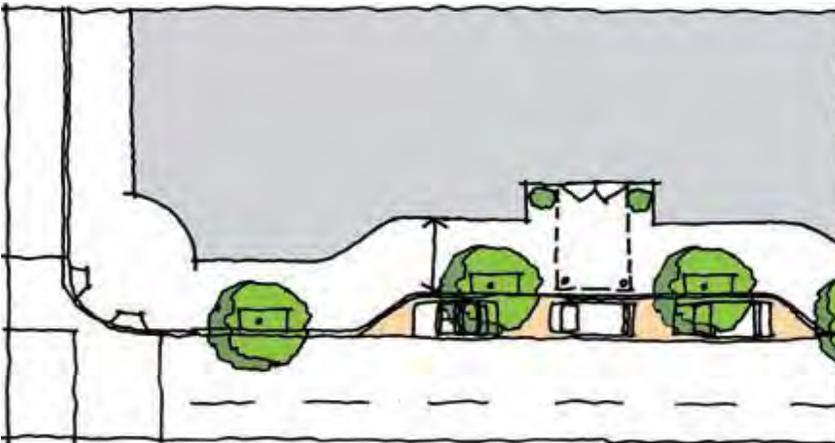
- > Drop-off, including residential, hotel and restaurant drop-off, shall be provided (1) within the off-street parking facilities using the parking access, (2) on a nonrequired private street, or (3) along the curb line of a public street or required private street where there is a full-time curbside parking lane, with no sidewalk narrowing.



1. Drop-offs occur within building envelope, with minimal obstruction to pedestrian activity.



2. Drop-offs along the curb of a public street or required private street.



3. Drop-offs can be inset on a nonrequired private street (no columns may be located in the walkway/path of travel).

#### 4.4.4 On-Site Open Space

Provision of on-site Open Space is essential to the creation of a green and pedestrian friendly network throughout the Specific Plan area. On-Site Open Space standards are included in Table 4-4. In addition, the standards in this section shall also apply as specified in Table 4-4. Refer to Section 4.5.4 (On-Site Open Space) for on-site open space design guidelines.

- A portion of the on-site open space shall be provided as required by the Municipal Code in a manner that contributes to the Specific Plan's open space network. Specifically:
  - > Improve and maintain an open space equivalent to 7% of the Project's site area at street level that is typically open to the public during daylight hours and preferably with a minimum street frontage of 20 feet, which may be continuous or segmented, and in accordance with Table 4-5 through Table 4-7 of this section.

- > If a single open space of more than 1 acre is provided, the publicly accessible open space requirement may be reduced to 5% of site area. Required publicly accessible open space need not be provided on the project site, but it shall be located within the Specific Plan area.
- Publicly accessible open space shall be designed so that it is:
  - > Directly accessible from the adjacent sidewalk
  - > On natural soil; not over a structure whenever possible
  - > At least 90% open to the sky, excluding shade structures or other elements approved by the City
  - > At least 75% landscaped, or in accordance with Table 4-7, unless the City approves a lesser percentage to accommodate paved recreational or other elements
  - > All paved areas are permeable or drain into a landscaped area where stormwater is collected and filtered
  - > It includes a mix of passive and active recreational facilities designed to serve residents, employees, and visitors to the Specific Plan area
  - > It includes at least one gathering place with a fountain or other focal element
- Ongoing maintenance and operation of the required open space shall be provided through a recorded covenant and ongoing public access through an easement.
- On-site common open spaces shall be comprised of one of (or a combination of) the following:
  - > **Parks and Squares.** Required publicly accessible open space will take the form of parks and public squares that are largely usable green space with active and passive recreational facilities. They will provide an open space network that is linked by streets, small slow vehicle paths, and paseos.

Typical neighborhood park



- > **Front Setbacks.** Building setbacks provide a transition between the public and private realm that benefits both building occupants and pedestrians.



Front setback open space prototype

- > **Paseos.** Paseos are extensions of the street grid located on private property. As outdoor passages devoted exclusively to pedestrians, they establish clear connections among streets, plazas and courtyards, building entrances, parking and transit facilities.



Paseo open space prototype

- > **Entry forecourts.** Entry forecourts announce the function and importance of primary building entrances. They should provide a clear, comfortable transition between exterior and interior space. Minimum dimension of a forecourt shall be 20 feet by 20 feet.



Entry forecourt open space prototype

- > **Courtyards.** Courtyards are common open space areas of a scale and enclosure that is conducive to social interaction at a smaller scale.

Typical residential courtyard



- > **Plazas.** Plazas are common open space areas typically amenable to larger public gatherings. They are readily accessible from the street, as well as active building uses.

Typical neighborhood plaza open space



- > **Corner Plazas.** Corner plazas should be appropriate in scale (intimate for residential, larger for commercial) and be programmed with specific uses (to provide outdoor dining for an adjacent restaurant, or small neighborhood gathering place featuring a public amenity). Over-scaled corner plazas are discouraged.

Typical corner plaza open space



- > **Rooftop Open Space and/or Terraces.** Roof terraces and gardens can augment open space and are especially encouraged in conjunction with hotels or residential uses.



Typical roof terrace open space

- On-site open space types shall be sited in relation to the street and permit public access during normal business hours as follows:
  - > Provide publicly accessible open spaces that may be shared and that provide pedestrian linkages as specified in Table 4-5 (Open Space-to-Street Relationship and Public Access Requirement).

<b>TABLE 4-5 Open Space-to-Street Relationship and Public Access Requirement</b>			
<i>Open Space Type</i>	<i>Location</i>	<i>Connection to Street</i>	<i>Public Access</i>
Parks and Squares	Enter at street level	Direct connection required	Required
Setbacks	Street level*	Visual access; may include public walkways	Per Figure 5-1
Paseos	Enter at street level	Direct connection required	Required
Entry Forecourts	Street level	Direct connection required	Required
Courtyards	Street level or above grade	Direct connection not required	Not required
Plazas and Corner Plazas	Enter at street level*	Direct connection required	Required
Roof Terraces	Above grade or rooftop	Direct connection not required	Not required

\* Minor elevation changes of up to 2 feet from sidewalk level are allowed, provided walkways and recreational facilities are accessible.

- Open space shall generally be contained along a minimum percentage of its perimeter by building and/or architectural features as specified in Table 4-6 (Containment of Open Space):

<i>Open Space Type</i>	<i>Minimum Containment</i>
Parks and Squares	2 sides*
Setbacks	1 side
Paseos	2 sides
Entry Forecourts	2 sides
Courtyards	3 sides
Plazas and Corner Plazas	1 side
Roof Terraces	1 side

\* Buildings may be located across a collector or local street (maximum 40-foot roadway width).

- Open Spaces shall incorporate amenities that facilitate outdoor activities such as standing, sitting, strolling, conversing, window-shopping and dining, including seating for comfort and landscaping for shade and aesthetics.
- Each open space type shall provide amenities in the form of a minimum planted area and number of seats as follows. Planters, planter boxes, and similar planting containers may count toward this requirement (refer to Table 4-7 [Landscaping and Seating]).



Seating is an essential element in most open spaces

<i>Open Space Type</i>	<i>Minimum Planted Area</i>	<i>Minimum Seating*</i>
Parks and Squares	75%	1 seat per 500 sf
Setbacks	Refer to Section 4.4.1	1 seat per 100 lf
Paseos**	30%	1 seat per 2,000 sf
Entry Forecourts	25%	1 seat per 500 sf
Courtyards	50%	1 seat per 500 sf
Plazas and Corner Plazas	25%	1 seat per 500 sf
Roof Terraces	25%	None required

\* Seats may be permanent or movable, accessible during normal business hours. Two linear feet of bench or seat wall equals one seat. A minimum of 2 seats should be provided in each location to allow for interaction.

\*\* Except where the paseo serves as a fire lane, in which case the paving should be turf block or similar permeability.

- Roof terraces shall incorporate trees and other plantings in permanent and temporary planters that will shade, reduce reflective glare, and add interest to the space. These spaces shall also include permanent and temporary seating that is placed with consideration to sun and shade, and other factors contributing to human comfort.

#### 4.4.5 Building Types

This section identifies permitted building types by land use and provides standards for each building type, to ensure that proposed development is consistent with the plan’s goals for building form, character, and quality. The building type structures included in this section will be allowed for new Multi Family, Live Work, Retail, Office, and Mixed Use development. Required Building Types per land use are specified in Table 4-4 (refer to Section 4.5.5 for Building Type Design Guidelines). Medical Office development is not required to comply with any of the building type designs in this section.

##### 1. Courtyard Podium

Courtyard Podium buildings can be arranged as townhouses over flats or flats over flats. These structures are typically built over ground level, subterranean, and/or semi-subterranean parking garages or a combination of the above with one or more façades lined at ground level by either housing or retail/office uses keeping the parking blocked from front street view. The main building mass is distributed around a central open courtyard space or a series of them. Habitable space may be single- or double-loaded and oriented to the street or an outdoor area.



##### 2. Urban Block

Urban block buildings are arranged as flats over flats. These structures are typically built next to surface parking and/or above parking garages with one or more façades lined at ground level by either housing, housing lobby/entrance areas, or retail/office uses keeping the parking blocked from front street view. Parking may also be provided on top of ground level retail/office uses and under residential and/or office space. No central courtyard is provided allowing for smaller lot size requirements. Common open space areas are typically designed as open roof terraces.



### 3. Liner Block



Liner Block buildings are designed around a structure such as a public parking garage. These buildings are designed for occupancy by retail, service, and/or office uses on the ground floor, with upper floors also configured for those uses or for residences. Liner Blocks are arranged as flats over flats with one or more façades lined at ground level keeping the parking blocked from front street view. Habitable space is single-loaded oriented to the street or an outdoor area.

### 4. Wrapped Block



Wrapped Block buildings are composed of one or more buildings, typically residential and/or mixed-use, that wraps around or encircles a detached multi-story parking garage. A wrapped building maintains an urban streetscape while providing suburban parking ratios. These structures are arranged as flats over flats and may be double loaded since the parking structure located in the center is detached and typically screened or buffered with landscape and open courtyard areas. Lot consolidation may be necessary to accommodate a Wrapped Block structure within the Specific Plan area.

## B. LOT SIZE

Table 4-8 (Lot Size Ranges by Building Type) illustrates lot size ranges allowed for each building type. Lot sizes for each building type are calculated based on typological characteristics like frontage type, accessibility, building mass configuration and open space.

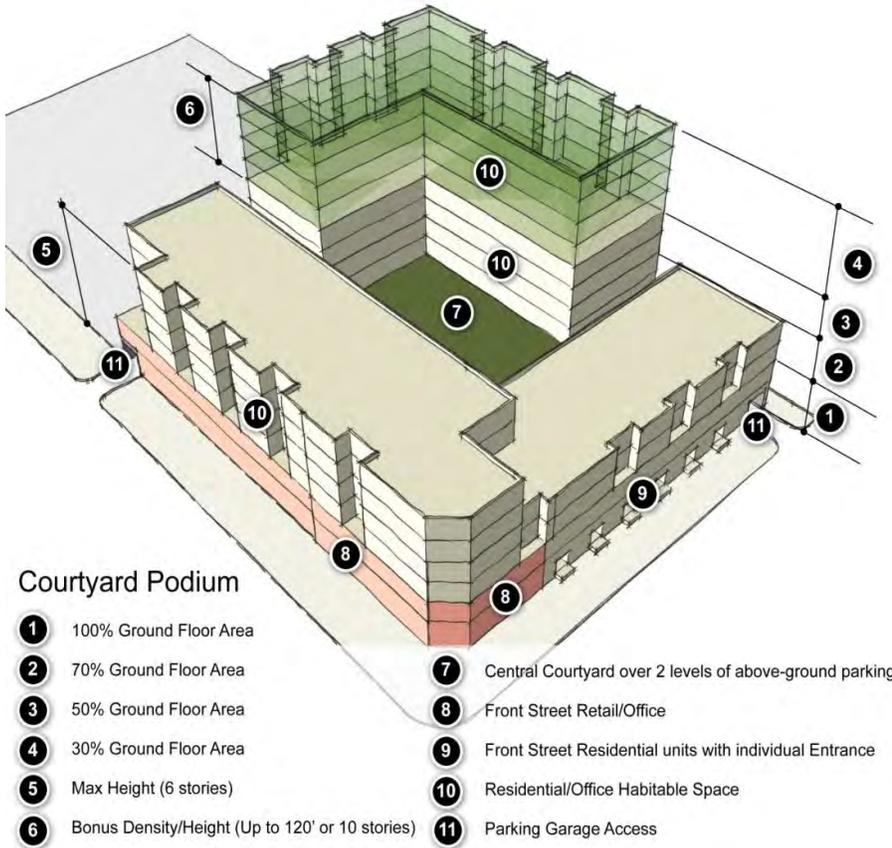
<i>Building Type</i>	<i>Lot Width (Min–Max)*</i>	<i>Lot Depth (Min)</i>
Courtyard Podium	100'–300'	150'
Urban Block	100'–350'	100'
Liner Block	100'–350'	150'
Wrapped Block	300'–500'	350'

Lots that are smaller than the minimum dimensions shown in this table are exempt from Building Type requirements.

\* Lot width on corner properties shall be measured perpendicular to the longest street-fronting side of the lot.

## C. BUILDING TYPE CHARACTERISTICS AND STANDARDS

### Courtyard Podium



Recessed walls and other building elements shown in this diagram are meant to be illustrative.

Double-loaded Courtyard Podium corner building diagram with parking under open courtyard area on a 300-foot-wide by 250-foot-deep lot. One street front is lined with retail/office space and the second street front is lined with residential units.

### Building Massing

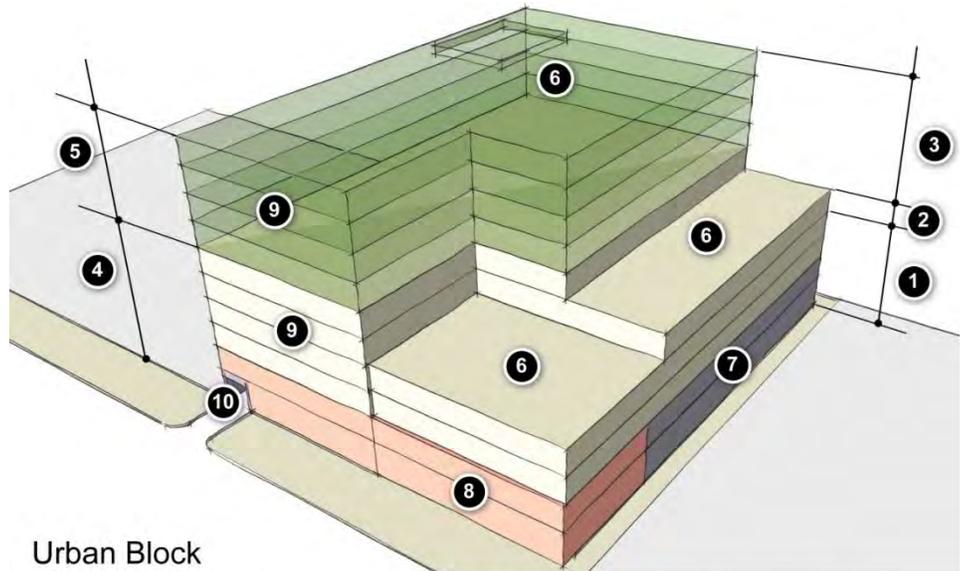
Building mass or shape is defined by the building's components, including the size of the footprint, number of stories, roof shapes, and upper level setbacks. The following standards require a minimum level of building mass articulation, resulting in buildings that are more visually attractive.

1. Courtyard Podium buildings shall be composed of two to six story structures. Additional building height may be allowed for projects that qualify for bonus density allowances. Refer to Section 4.3.4 (Minimum and Maximum Densities) and Table 4-4 (Development Standards) for density bonus criteria.

- Buildings shall be designed with components of varying height as indicated by Table 4-9 (Allowed Massing by Story—Courtyard Podium).

TABLE 4-9 Allowed Massing by Story—Courtyard Podium	
Story No.	Maximum Ratio of Each Story in % of Ground Floor Area
1 <sup>st</sup> and 2 <sup>nd</sup> Stories	100%
3 <sup>rd</sup> and 4 <sup>th</sup> Stories	70%
5 <sup>th</sup> and 6 <sup>th</sup> Stories	50%
7 <sup>th</sup> Story and Higher	30%

### Urban Block



#### Urban Block

- 1 100% Ground Floor Area
- 2 80% Ground Floor Area
- 3 60% Ground Floor Area
- 4 Max Height (6 stories)
- 5 Bonus Density/Height (Up to 120' or 10 stories)
- 6 Roof Terrace Areas Oriented South
- 7 Above-Ground Parking
- 8 Front Street Retail/Office
- 9 Residential/Office Habitable Space
- 10 Parking Garage Access

Recessed walls and other building elements shown in this diagram are meant to be illustrative.

Urban Block building diagram showing parking behind retail/office uses on a 200-foot-wide by 280-foot-deep lot.

### Building Massing

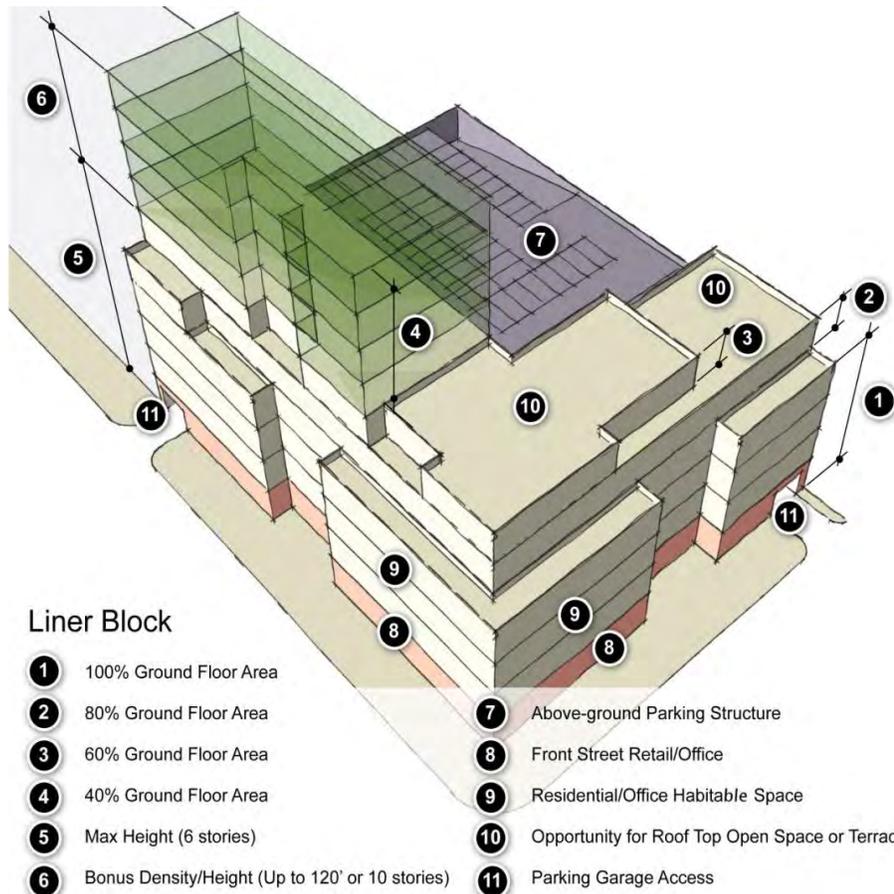
- Urban Block buildings shall be composed of four to six story structures (except for 100% Retail uses, which may be one or two stories high). Additional building height may be allowed for

projects that qualify for bonus density allowances. Refer to Section 4.3.4 (Minimum and Maximum Densities) and Table 4-4 (Development Standards) for density bonus criteria.

- Buildings shall be designed with components of varying height as indicated by Table 4-10 (Allowed Massing by Story—Urban Block).

TABLE 4-10 Allowed Massing by Story—Urban Block	
Story No.	Maximum Ratio of Each Story in % of Ground Floor Area
1 <sup>st</sup> through 4 <sup>th</sup> Stories	100%
5 <sup>th</sup> Story	80%
6 <sup>th</sup> Story and Higher	60%

### Liner Block



Recessed walls and other building elements shown in this diagram are meant to be illustrative.

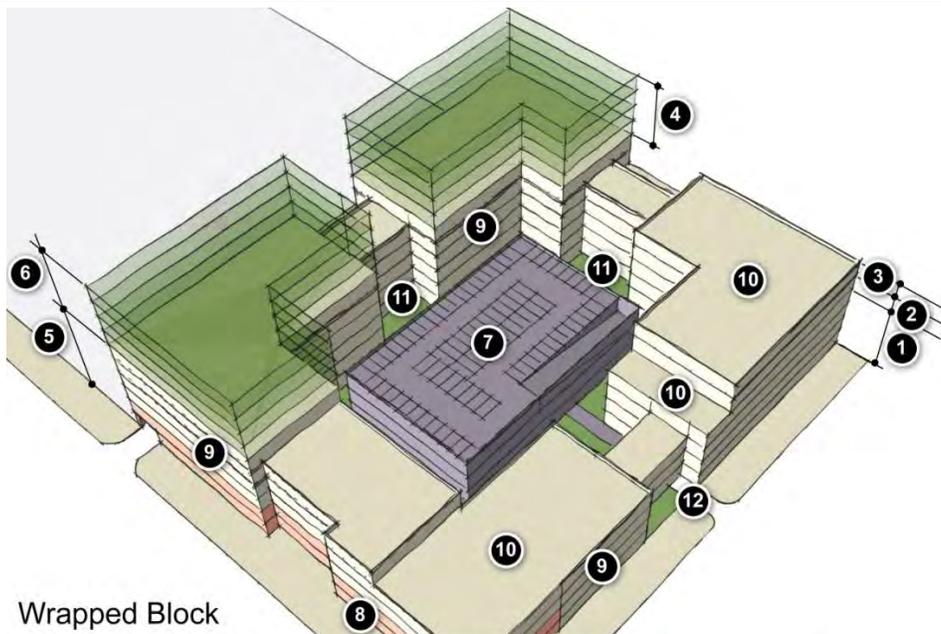
Liner Block diagram on a 250-foot-wide by 200-foot-deep lot. This diagram shows double-loaded corner configuration around a parking garage and street front retail/office uses.

**Building Massing**

1. Liner Block buildings shall be composed of three- to six-story structures (except for 100% Retail uses, which may be one or two stories high). Additional building height may be allowed for projects that qualify for bonus density allowances. Refer to Section 4.3.4 (Minimum and Maximum Densities) and Table 4-4 (Development Standards) for density bonus criteria.
2. Buildings shall be designed with components of varying height as indicated by Table 4-11 (Allowed Massing by Story—Liner Block).

<b>TABLE 4-11 Allowed Massing by Story—Liner Block</b>	
<i>Story No.</i>	<i>Maximum Ratio of Each Story in % of Ground Floor Area*</i>
1 <sup>st</sup> – 4 <sup>th</sup> Stories	100%
5 <sup>th</sup> Story	80%
6 <sup>th</sup> Story	60%
7 <sup>th</sup> Story and Higher	40%
* Parking garage ground floor areas shall not count toward this calculation.	

## Wrapped Block



### Wrapped Block

- |   |   |
|---|---|
| ① 100% Ground Floor Area                          | ⑦ Detached Parking Structure Garage in Center                       |
| ② 80% Ground Floor Area                           | ⑧ Front Street Retail   |
| ③ 60% Ground Floor Area                           | ⑨ Residential/Office Habitable Space                                |
| ④ 40% Ground Floor Area                           | ⑩ Opportunity for Roof Top Open Space or Terraces                   |
| ⑤ Max Height (6 stories)                          | ⑪ Open Space Areas between Main Building Mass and Parking Structure |
| ⑥ Bonus Density/Height (Up to 120' or 10 stories) | ⑫ Parking Garage Access   |

Recessed walls and other building elements shown in this diagram are meant to be illustrative.

Wrapped Block building diagram showing detached parking structure in the center and open space/main building mass arranged around it. This diagram shows double-loaded building structures and fits in a 350-foot-wide by 400-foot-deep lot.

### Building Massing

1. Wrapped Block buildings shall be composed of three to six story structures. Additional building height may be allowed for projects that qualify for bonus density allowances. Refer to Section 4.3.4 (Minimum and Maximum Densities) and Table 4-4 (Development Standards) for density bonus criteria.
2. Buildings shall be designed with components of varying height as indicated by Table 4-12 (Allowed Massing by Story—Wrapped Block).

<i>Story No.</i>	<i>Maximum Ratio of Each Story in % of Ground Floor Area*</i>
1 <sup>st</sup> – 4 <sup>th</sup> Stories	100%
5 <sup>th</sup> Story	80%
6 <sup>th</sup> Story	60%
7 <sup>th</sup> Story and Higher	40%

\* Parking garage ground floor areas shall not count toward this calculation.

#### 4.4.6 Streetscape Standards

This section defines streetscape design standards for improvements to be undertaken in conjunction with development projects. The standards refer to the plans, cross sections, and descriptions in Section 3.6 (Open Space and Streetscape Improvement Plan). Applicability of the following standards by planning district (Figure 4-2) is specified in Table 4-4.

##### A. STREET CONFIGURATION

Figure 3-18A through Figure 3-26 illustrate the intended design along each street. These plans and cross sections illustrate typical conditions along each street. Complete plans of streets on which curb lines will be relocated in the future are in Chapter 3 (Policies and Development Plans). Figure 3-4A through Figure 3-11B depict existing as well as proposed future conditions to illustrate required changes to achieve the vision for the Specific Plan area. The description, plans, and cross sections for each street address:

- Illustrative ROW and lane configuration, including landscaped medians
- Recommended multi-use trail along Oso Creek for equestrians, bicycles, and pedestrians
- Minimum required sidewalk width, which may be a combination of public right-of-way (which may require a dedication) and easement for sidewalk purposes
- Required sidewalk configuration, which typically includes an 8-foot-wide continuous landscaped parkway and a 6- to 10-foot-wide paved walkway
- Required setback width, which is a function of the adjacent ground floor use
- Illustrative setback treatment, which is also a function of the adjacent ground floor use. The cross sections in Figure 3-18A

through Figure 3-26, as well as Figure 4-11, illustrate the setback treatment. Along Forbes Road and other pedestrian-oriented retail streets, the cross sections show the required treatment adjacent to the predominant ground floor use on each street segment.



Illustrative sketch of the east side of Forbes Road and other pedestrian-oriented streets with required streetscape improvements

### **Improve street rights-of-way.**

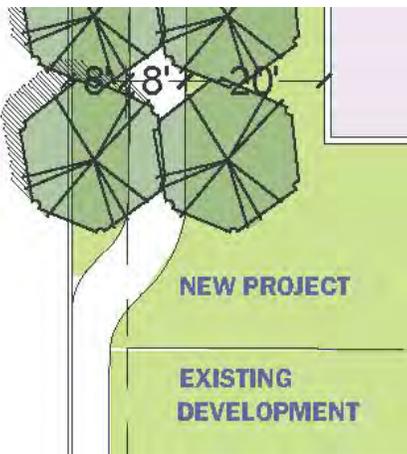
1. Each project shall improve the street right-of-way adjacent to the Project as described and illustrated in Figure 3-18A through Figure 3-26.
2. All utility lines within the public rights-of-way adjacent to the Project and on the Project site shall be undergrounded.
3. All utility boxes, including traffic control, electrical, phone, and fiber optics, shall be undergrounded, unless the City approves an above-grade box due to extenuating circumstances.



A mix of native and other drought-tolerant plantings



Parkways/walkways can meander; seating should be provided in the setback



Transition from existing narrow sidewalk to new parkway/walkway

## B. LANDSCAPE TREATMENT ALONG STREETS AND OTHER RIGHTS-OF-WAY

### Use appropriate plant materials to reinforce the character of the Specific Plan area.

The public realm landscape will link the Specific Plan Area both to the natural environment and to the rest of the City. It builds on the landscape plan adopted in the 1999 Specific Plan, as well as the subsequent design effort for landscaping the rail ROW along the west side of Camino Capistrano.

Through streets, including Crown Valley Parkway and Camino Capistrano, align more with the City as a whole and, therefore, will retain a landscape character similar to that of the larger City. Primarily local-serving streets, including Forbes Road, Cabot Drive, Getty Drive, and Cape Road, will develop a character unique to the Specific Plan Area, with a strong connection to Oso Creek and surrounding grassland environment.

1. New street trees of the species specified in Section 3-6 shall be planted in the parkway or setback. For new streets for which species are not shown, a species/cultivar that will achieve a mature height and spread of 35 feet within 10 years shall be planted at an average spacing of 30 feet on center.
2. Shrubs and groundcover along the primarily local-serving streets shall be largely natives and their cultivars.

### Design the parkways and setbacks to accommodate and support large street trees and provide a buffer from traffic.

1. Each Project shall provide continuous landscaped parkways and/or setbacks that are minimum 8 feet wide, as shown in Figure 3-18A through Figure 3-26, except where parkways would conflict with bus stops. The continuous landscaped parkways and setbacks should be designed to collect and retain or treat runoff from, at a minimum, the sidewalk and, if approved by the City, adjacent on-site, ground level open space. As shown in Figure 4-9, provide slope and screen landscaping along other rights-of-way. Refer to Figure 4-10 for suggested groundcover and shrub massing to achieve screening in setbacks.
2. The roots of trees planted in the parkway or setback shall not be restricted by concrete curbs, root barriers or other means, so that roots may extend throughout the parkway and support a large, healthy tree canopy.

Relatively dense native trees and shrubs can provide buffers along the west edge of the railroad ROW. Less dense native trees and shrubs can be used along the freeway frontage. Trees should be used more sparingly, leaving open spaces between, along the freeway frontage to maintain views to the Specific Plan area.

**Tree Palette**

Evergreen Trees Adjacent to Railroad ROW

- Tecate Cypress
- Cupressus forbesii*

- California Bay
- Umbellularia californica*

More Open Trees Along Freeway

- Blue Oak
- Quercus douglasii*

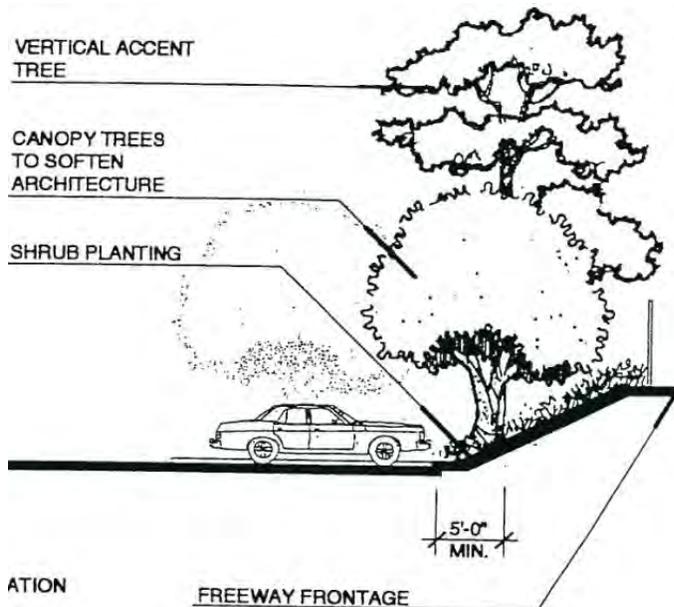
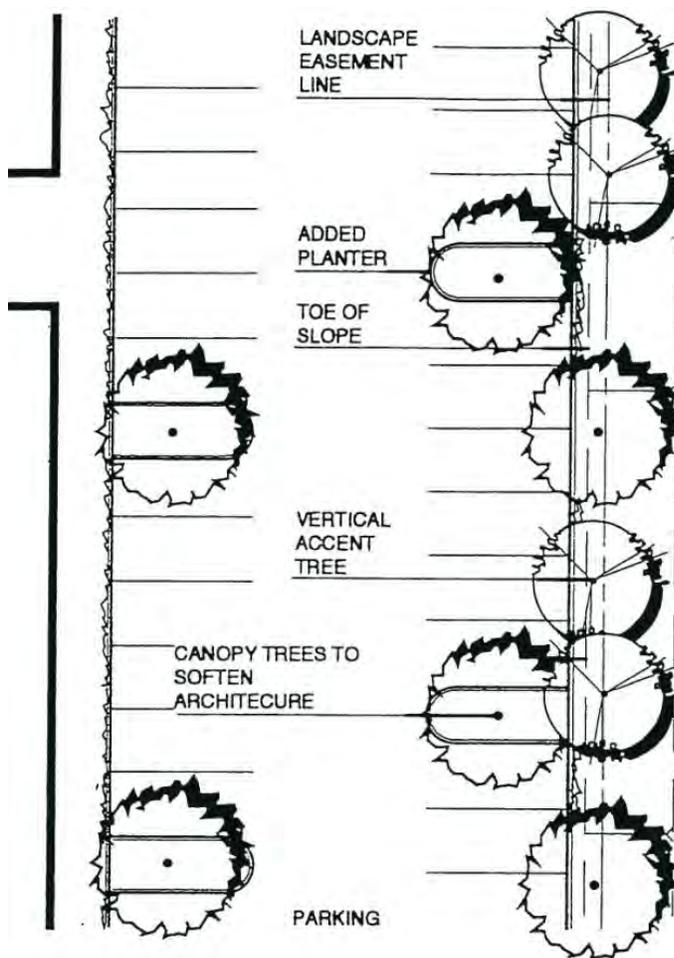
- California buckeye
- Aesculus californica*

- Shore Pine
- Pinus contorta*

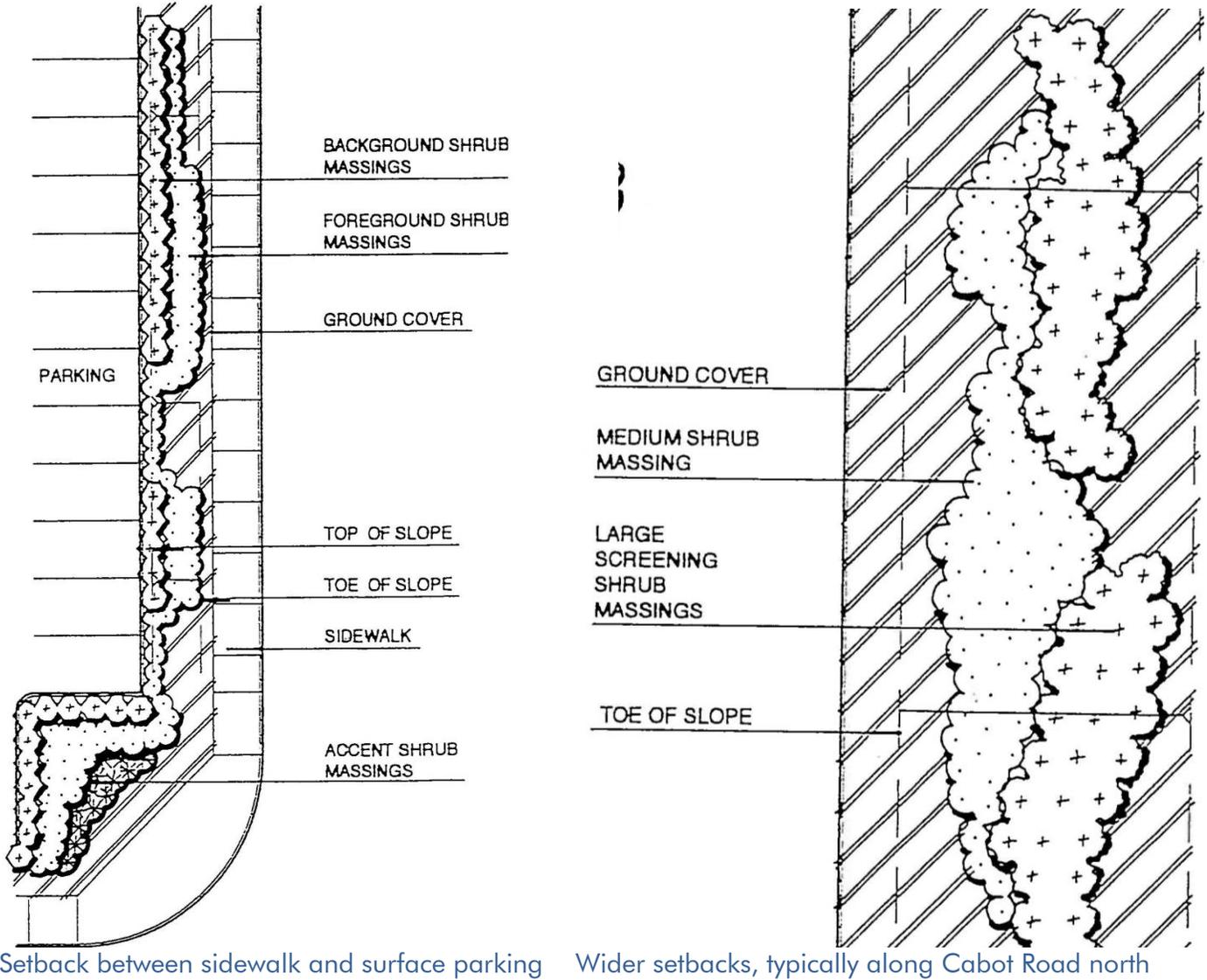
- Flowering Ash
- Fraxinus depetala*

Vertical Accent Trees Along Freeway

- California Sycamore
- Platanus racemosa*



**FIGURE 4-9 West Side of Railroad Right-of-Way and West Side of Freeway: Typical Plan Views and Cross Sections**



Setback between sidewalk and surface parking Wider setbacks, typically along Cabot Road north

**FIGURE 4-10 Typical Setback Landscaping Except on Forbes Road and Other Pedestrian-Oriented Retail Streets**

3. If parkways and setbacks are designed to collect stormwater from the street as well as from the sidewalk, they shall be designed according to Public Works standards.
4. Where a new Project is adjacent to an existing sidewalk on Forbes Road the walkway and parkway shall transition as shown in the adjacent diagram.

### Design parkways and setbacks to optimize tree health.

1. Plant street trees of the species/cultivars specified in Section 3-6 in conjunction with each project. In-lieu fees are not allowed.
2. Trees shall be spaced from one another as specified in Section 3-6
3. 36-inch minimum box trees shall be planted.
4. Parkway shall be planted with drought-tolerant groundcover or perennials at least 18 inches but not more than 3 feet tall, except within 3 feet of tree trunks, where the surface should be mulched.
5. Landscaped parkways and tree wells shall be irrigated with an automatic irrigation system. In-line drip irrigation is preferred. Spray heads or bubblers may also be used provided they adequately irrigate trees (minimum of 20 gallons per week dispersed over the root zone) and do not directly spray the tree trunks.

### Provide maintenance appropriate to native and drought-tolerant landscaping.

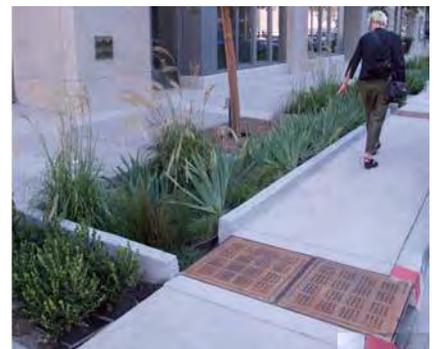
1. Remove all existing exotic weedy plants as identified by the California Invasive Plant Council ([www.cal-ipc.org](http://www.cal-ipc.org)).

### C. WALKWAYS

On Forbes Road, Getty Drive, Cape Road, and future pedestrian-oriented retail streets, the sidewalk is divided into two parts (as depicted in Section 4.4.1): the parkway, which is adjacent to the curb, landscaped and designed to accommodate trees and provide a buffer from the street, and the walkway, as illustrated below. On other streets, the walkway is along the curb and street trees and landscaping are in a required setback on adjacent private property.



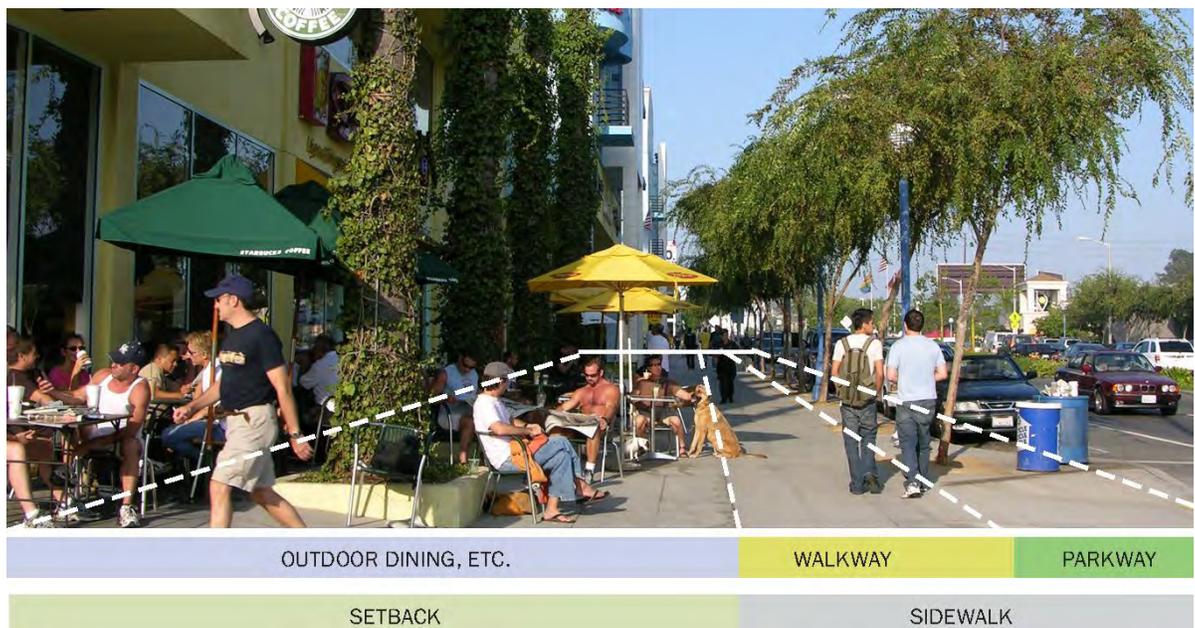
All continuous landscaped parkways collect stormwater runoff from the sidewalk



In addition, they can be designed to filter stormwater run-off from street, per BOE Green Street standards; if there is a raised curb around the parkway and curbside parking as in this example, the curb access strip must be wider than 2 feet

**Provide adequate width for improvements based on adjacent ground floor use.**

1. Walkways shall be provided as specified in Figure 3-18A through Figure 3-26. The required walkway may be located as shown or it may be located partially within the first 8 feet of the required setback. That is, the sidewalk may undulate within the required easement and setback.
2. Structures shall not project over or under the required easement or public right-of-way to allow for stormwater infiltration, tree canopies, and soil volume for tree roots.
3. A 2-foot-wide paved access zone shall be provided next to the curb where there is curbside parking.
4. A minimum 6-foot-wide clear and continuous path of travel walkway shall be maintained where outdoor dining occurs.



Example showing the parkway along the curb, the walkway, and use of the setback for outdoor dining

**D. SETBACK TREATMENT ON PEDESTRIAN-ORIENTED STREETS**

Figure 4-4 and Figure 4-5 establish (1) the required setbacks from the back of walkway to building street walls and (2) treatment of the required setbacks for the east side of Forbes Road. Figure 4-8 specifies the same for any new pedestrian-oriented streets.

### Provide setback treatments appropriate to districts and adjacent uses.

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1. Provide setback treatments as specified in Figure 4-4 through Figure 4-8 and Figure 4-11, except where the setback is part of a larger usable open space.
2. Adjacent to ground floor retail, the ground floor street wall may be set back farther to accommodate dining or similar activities.

### Treat setbacks appropriately given the adjacent land use and district.

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1. Adjacent to retail, the required setback area shall be at least 75% hardscape and may be used for outdoor dining and other commercial activities.
2. Adjacent to live-work space or professional office space, at least 50% of the required setback area shall consist of landscaping.
3. Adjacent to ground-floor residential units with individual entries or residential common areas (lobbies, recreation rooms, libraries, or other active uses), the required setback area shall be at least 75% landscaped and may include: walkways, porches, raised planters and other solid walls up to 3 feet above sidewalk elevation, and transparent fences (e.g., wrought iron, tubular steel, glass) up to a height of 4 feet above sidewalk elevation.
4. Surface parking shall not be located in the setback area.

### Don't waste valuable street frontage on "back-of-house" uses.

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1. Loading docks, electrical transformers, mechanical and other equipment shall be located so that they are not in the setback area or visible from a public street.
2. Enclosed stairs, storage spaces, blank walls, and other elements that are not pedestrian-oriented shall be located more than 100 feet from the corner of any public or required private street and, to the extent feasible, so they are not visible from the street.



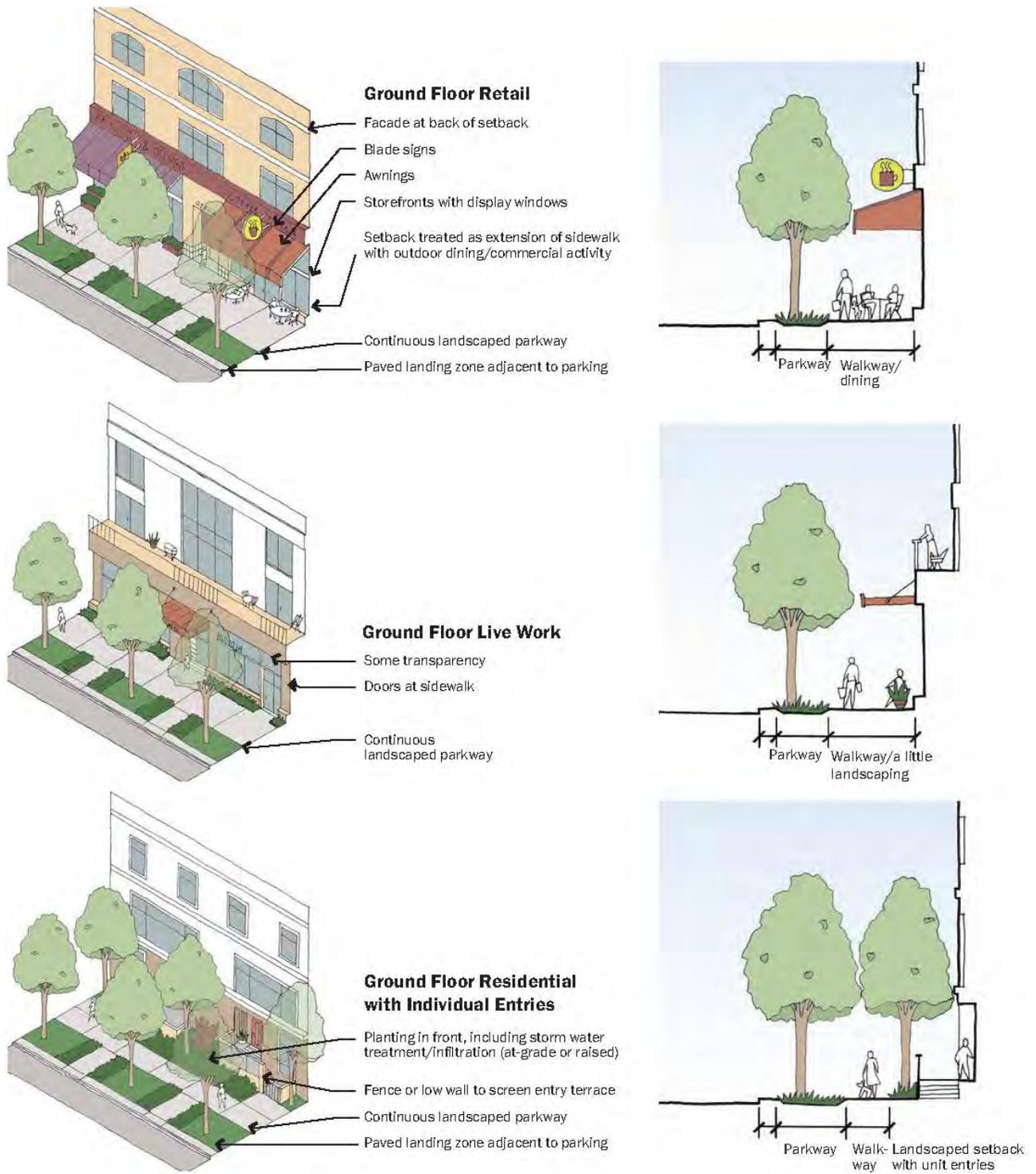
Zero setback with ground-floor retail



A small setback with a little landscaping next to professional office or live-work space



Housing with front yards (setbacks) and secondary entrances along the sidewalk



**FIGURE 4-11 Setback Treatments on Forbes Road, Other Pedestrian-Oriented Retail Streets, and Local Streets Vary with Ground Floor Use**

## E. CURB EXTENSIONS AND CROSSWALKS

1. Curb extensions shall be provided at all corners and midblock crossings on Forbes Road and local streets, except at the intersection of Forbes Road with Crown Valley Parkway.
2. Ladder or zebra striping shall be installed at all crosswalks. Other enhanced paving may be approved provided it is as visible as ladder striping and is regularly maintained by the Applicant or City.

## F. STREET LIGHTS AND ELECTRICAL SERVICE

On most streets in the Specific Plan area, there are two types of street lights: roadway lights (“street lights”) and pedestrian-scale lights (“pedestrian lights”). Street lights provide illumination of both the roadways and sidewalks to the levels required by Public Works for safety and security. Pedestrian lights are ornamental and supplement the illumination provided by the street lights. Pedestrian lights contribute to the pedestrian scale of the street and add a warm glow of yellow light on the sidewalk. On local or private streets, which are narrower, a single “hybrid” fixture can illuminate both the roadway and sidewalk.

1. On local or private streets, hybrid street lights adjacent to the curb 50 to 60 feet on center shall be installed.
2. On all other streets, roadway lights adjacent to the curb 100 to 120 feet on center and pedestrian street lights in the parkway or setback 50 to 60 feet apart and offset by 25 to 30 feet from the roadway lights shall be installed.
3. All light sources shall provide a warm (yellow, not blue) light and shall be LED or a future more energy-efficient technology.
4. All optic systems shall be cut off with no light trespass into the windows of residential units.
5. On Forbes Road, provide adequate electrical service in the setback area to energize seasonal lighting and other special event needs. At a minimum one outlet adjacent to each tree in the setback shall be provided.
6. Street lights for the Specific Plan area shall be specified by the City. A family of complementary street lights is recommended.



Street light examples



Pedestrian light examples

## G. STREET FURNITURE

Provide street furniture along pedestrian-oriented streets.

1. Provide 2 seats, 1 trash receptacle, and bicycle parking for 5 bicycles in the public ROW or adjacent setback for every 150 feet of project frontage. The furniture design and location shall be specified as part of the project application process.



Seating and trash receptacles



Bicycle racks

### 4.4.7 Signage Standards by Land Use

The signage standards in this section are intended to appropriately limit the placement, type, size, and number of signs allowed within a particular project within the Specific Plan area. The standards in this section are arranged by land use classification as illustrated in Table 4-13.



TABLE 4-13 Signage Standards	
RETAIL COMMERCIAL (RC) AND MIXED USE (MU) PLANNING DISTRICTS	
GENERAL STANDARDS (Applies to All Signs in RC & MU Planning Districts)	
<b>Signage Concept</b>	Signs in areas designated as either Retail Commercial (RC) or Mixed Use (MU) within the Laguna Niguel Gateway Specific Plan are intended to establish a lively, contemporary pedestrian oriented atmosphere. The mounting height of these signs shall be lower than in typical retail centers to contribute to the activity at the pedestrian level. The use of exposed neon in reverse channel and open channel letters, as well as dimensional shapes and logos is encouraged. The signs shall be engaging and multi-layered, but tasteful in character. Creativity, especially with respect to type face, lettering style, colors, materials, sign shape, lighting, texture, etc., is encouraged in sign design in the RC and MU districts.
<b>Colors</b>	Sign colors in the RC or MU districts shall be clear and vibrant. Exciting color combinations with high contrast accents are encouraged; however, day glow colors are not permitted. Sign colors shall read well against the building facade colors.
<b>Materials and Finishes</b>	Unusual combinations of metals, carved wood, rigid foams, meshes, perforated materials, tiles, and painted and glossy finishes are encouraged.
<b>Required Sign Review</b>	All projects in the RC and MU districts must apply for a Site Development Permit in conformance with Section 9-1-150 of the adopted Laguna Niguel Municipal Code to establish a sign program. The Site Development Permit application package shall include, as a minimum, the following: <ul style="list-style-type: none"> <li>■ A Sign Plan depicting the location of each and every proposed sign.</li> <li>■ Sign dimensions (height, width) for each and every proposed sign.</li> <li>■ Sign area of each sign (total sign area, not just message area, in square feet).</li> <li>■ Sketches and elevations depicting proposed sign design, including colors &amp; materials, copy (type face, lettering size, logos, etc.), architectural embellishments, etc.</li> <li>■ Color &amp; building materials palette(s) for all proposed signs.</li> <li>■ Any additional items identified by City Staff as being important to understanding the design intent of the proposed sign(s).</li> </ul>
PRIMARY PROJECT IDENTIFICATION SIGNS	
<b>Number of Signs</b>	No more than one Primary Project Identification Sign may be erected per project. A project shall be defined as any retail, support/service commercial, highway commercial, or mixed use development consisting of no less than 75,000 square feet of leasable or usable floor area, excluding covered or structured parking areas, covered loading/unloading areas, and covered trash enclosures. Projects not qualifying for a Primary Project Identification Sign are eligible for one freestanding Monument/Ground Sign. Projects consisting of no less than 120,000 square feet of leasable or usable floor area may erect one Primary Project Identification Sign and one Monument/Ground Sign. Projects consisting of no less than 150,000 square feet of leasable or usable floor area may erect one Primary Project Identification Sign and <u>either</u> a Secondary Project Identification Sign <u>or</u> a Monument/Ground Sign (but not both).
<b>Locations(s)</b>	The Primary Project Identification Sign shall be located close to the street right-of-way, but must not block cars entering or exiting the retail center. In no case shall the Primary Project Identification Sign be located less than five (5) feet from any public street right-of-way. Sightlines for the safe operation of motor vehicles must be preserved and not blocked by any signs.
<b>Maximum Sign Dimensions</b>	20 feet (height) x 10 feet (width) for entire sign structure, not just the message area. If the sign is erected in a raised planter box, the sign height, inclusive of the planter height, shall not exceed 20 feet. Architectural embellishments are permitted, and may extend out a maximum of 5 feet in any direction from the main body of the sign.

<b>TABLE 4-13 Signage Standards</b>	
<b>Design and Landscaping</b>	Pole signs are prohibited. Each sign shall incorporate architectural elements and embellishments and must be constructed of at least two different building materials. Each sign shall be installed within a landscaped area of not less than 150 square feet (raised planter boxes are acceptable, provided that the sign height, inclusive of the planter box height, does not exceed 20 feet in height).
<b>Text (Copy)</b>	Each Primary Project Identification Sign shall include a list of some or all of the major tenants located within the project; provided, however, that no more than six tenants shall be permitted on any one sign. Tenant logos and tenant names (in any color and type style) are permitted. In addition, the name, logo, and address of the project or shopping center are permitted as well. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the major buildings on-site. Multiple materials are encouraged for use on each sign (a minimum of two different materials are required). Signs shall incorporate vibrant colors that create a dynamic sense of excitement.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is encouraged.
<b>SECONDARY PROJECT IDENTIFICATION SIGNS</b>	
(Permitted Only When Used in Conjunction with Major Project Center Identification Signage within the Same Retail Center or Project)	
<b>Number of Signs</b>	Secondary Project Identification Signs are permitted only in retail centers and other projects that front on two or more public streets. Retail centers and other projects that front on only one public street are not eligible for Secondary Project Identification Signage. No more than one Secondary Project Identification Sign is permitted per street frontage. In addition, a project must contain at least 150,000 square feet of leasable or usable floor area (excluding covered or structured parking areas, covered loading/unloading areas, and covered trash enclosures) to qualify for both Primary and Secondary Project Identifications Signs. In no case shall any project have more than two Secondary Project Identification Signs. Sign may be double sided.
<b>Location(s)</b>	Secondary Project Identification Sign(s) shall be located for maximum visibility, usually adjacent to the street right-of-way. No more than one Secondary Project Identification Sign is permitted per street frontage. Signs shall be located close to the street right-of-way (at driveways), but must not block cars entering and exiting the retail center. Sightlines for the safe operation of motor vehicles must be preserved and not blocked by any signs.
<b>Maximum Sign Dimensions</b>	50 square feet for the entire sign structure, not just the message area. In no case shall sign exceed ten feet in height.
<b>Design and Landscaping</b>	Pole signs are prohibited. Each sign shall incorporate architectural elements and embellishments and must be constructed of at least two different building materials. Simulation motion graphics (no words allowed) are permitted, and shall be limited to three lighting sequence changes per minute. Each sign shall be installed within a landscaped area of not less than 100 square feet.
<b>Text (Copy)</b>	The name and address of the shopping center and tenant names are permitted on Secondary Project Identification Signs. The tenant names shall be all one color, although each tenant may elect to use their own particular type style. Tenant logos are <u>not</u> permitted, although the logo for the shopping center or project is allowed. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the Primary Project Identification Sign, as well as the architecture on-site. Multiple materials are encouraged for use on each sign (a minimum of two different materials are required). Signs shall incorporate vibrant colors that create a dynamic sense of excitement.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is encouraged.

TABLE 4-13 Signage Standards	
MONUMENT/GROUND SIGNS	
<b>Number of Signs</b>	One freestanding Monument/Ground Sign is permitted for each single-tenant pad. Monument/Ground Signs shall not be permitted on projects qualifying for Primary Project and Secondary Project Identification Signs. Monument/Ground Signs are intended for use on freestanding pads only.
<b>Location(s)</b>	The Monument/Ground Sign shall be located for maximum visibility, close to the street right-of-way. However, the sign must not block cars entering or exiting the site; sightlines for the safe operation of motor vehicles must be preserved. The proposed location of the Monument/Ground Sign on-site shall be approved by the City as part of a Site Development Plan (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Size</b>	The primary body of each Monument/Ground Sign shall not exceed four feet in height, although architectural accents and embellishments may extend an additional foot in height for a maximum total sign height of up to five feet. The total area of each sign (including architectural accents and embellishments) shall not exceed 24 square feet.
<b>Text (Copy)</b>	Only the tenant name, tenant logo, and the building address are permitted on each Monument/Ground Sign. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the architecture on-site.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is permitted.
WALL SIGNS	
<b>Number of Signs</b>	The total number of Wall Signs shall be no greater than the number of tenants in said building, with the exception that: <ul style="list-style-type: none"> <li>■ Tenants with a floor area of up to 20,000 sq. ft. may have up to two wall mounted signs with no more than one sign allowed per building face.</li> <li>■ Tenants with a floor area of greater than 20,000 sq. ft. may have up to one wall mounted sign for each building face, provided that no building shall be deemed to have more than three building faces.</li> </ul>
<b>Location(s)</b>	Signs shall be located to promote visibility. The location(s) of each and every proposed wall mounted sign(s) as it appears on the building(s) shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code). Wall Signs shall not project above the building cornice, parapet, or roof line.
<b>Size (Tenants up to 20,000 sq. ft.)</b>	The permitted size of the wall-mounted signs for each tenant shall not exceed 50 sq. ft. in area per sign. Signs shall not exceed five feet in height.
<b>Size (Tenants 20,000 sq. ft. and greater)</b>	The permitted size of the wall-mounted signs for each tenant shall be calculated as follows: One square foot of signage shall be permitted for each linear foot of building frontage on the side of the building where the main entrance is located, provided that no sign shall exceed 60% of the building frontage length. (For example, if a building has a 100-foot-long front facade, then each permitted wall mounted sign could be up to 60 feet in length.) For buildings with multiple tenants, the building frontage shall be defined as the portion of the front facade that relates to that particular building tenant. Signs shall not exceed five feet in height.
<b>Height</b>	Wall Signs shall not project above the building cornice, parapet, or roof line.
<b>Design</b>	Individual channel letters are recommended for Wall Signs. Can signs are prohibited. All Wall Signs shall be built as 3-Dimensional (3-D) signs may, if desired, incorporate 3-D elements. No sign, including any light box or other structural part, shall project more than 18" from the building face.

<b>TABLE 4-13 Signage Standards</b>	
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the wall or building to which the sign is attached. Vibrant colors are encouraged. Graphic elements and/or dimensional logos shall be incorporated into signs.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is encouraged. No can lighting is permitted.
<b>BLADE SIGNS</b>	
<b>Number of Signs</b>	No more than one blade sign shall be permitted per building entry (excludes employee, service, and emergency entries).
<b>Location(s)</b>	Signs shall be located over or adjacent to the sidewalk from which the primary building entry occurs.
<b>Size</b>	Signs shall not be larger than four square feet in size for the entire sign structure, not just the message area.
<b>Text (Copy)</b>	Copy shall be limited to the tenant's name and logo only. Tenants may elect to use their own particular type and logo colors and styles. Tenant's logo may be used in addition to or in place of the text. Descriptions of services or additional copy on Blade Signs are not permitted.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the wall or building to which the sign is attached. Vibrant colors are encouraged. Graphic elements and/or dimensional logos shall be incorporated into signs.
<b>Lighting</b>	Signs shall be externally illuminated. No exposed neon lighting is permitted on Blade Signs.
<b>MISCELLANEOUS SIGNS</b>	
<b>Canopy/Awning Signs</b>	Awnings and canopies are permitted, but may not be used as signage (no text or logos permitted). However, blade signs may be erected under awnings and canopies.
<b>Window &amp; Door Signs</b>	White vinyl letters are permitted on doors only. Letters shall not exceed 3 ft in height. No lettering or logos are permitted on windows.
<b>Directional Safety, Warning, or Information Signs (On Private Property Only)</b>	Directional/Information Signs shall not exceed seven feet in height. Signs may be double sided. The entire sign (including message area and sign structure) shall not exceed 20 square feet in area. Directional/Information Signs shall be externally lighted. No internal or neon lighting is permitted. The exact numbers and locations of proposed Directional Safety, Warning, or Information Signs shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Directional Safety, Warning, or Information Signs (within Public Rights-of-Way Only)</b>	Directional/Information Signs within public rights-of-way may consist of either freestanding kiosks or pole-mounted signs or banners/signs erected on light standards. All proposed public Directional Safety, Warning, or Information Signs shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>COMMUNITY SERVICE (CS) PLANNING DISTRICTS</b>	
<b>GENERAL STANDARDS</b>	
<b>Signage Concept</b>	Signs in the Community Service (CS) district within the Laguna Niguel Gateway Specific Plan are intended to establish signage consistency within the Community Service area. The mounting height of these signs will be higher than typical in other areas of the Specific Plan, because of the need for freeway visibility. Since the Community Service area is located at a lower grade than the I-5 Freeway, signs must be taller than normal in order to be visible. The use of exposed neon in reverse channel and open channel letters is permitted in limited amounts. Signs shall be simple and avoid too much clutter. Moving/animated signs and electron message boards on signs are strictly prohibited. Signs may incorporate corporate logos.

<b>TABLE 4-13 Signage Standards</b>	
<b>Colors</b>	Sign colors in the Community Service (CS) district shall be clear and simple in shape and concept. Sign colors shall read well, but overly vivid colors and neon colors are discouraged.
<b>Materials and Finishes</b>	Signs shall be constructed of durable materials such as metals, concrete, or heavy duty plastic. Wood is permitted only for limited areas of signs such as architectural embellishments or the sign frame. If wood is used, all wood surfaces shall be painted, glossy finishes are encouraged.
<b>Required Sign Review</b>	<p>All projects in the Community Service (CS) district must apply for a Site Development Permit in conformance with Section 9-1-150 of the adopted Laguna Niguel Municipal Code to establish a project sign program. The Site Development Permit application package shall include, as a minimum, the following:</p> <ul style="list-style-type: none"> <li>■ A Sign Plan depicting the location of each and every proposed sign</li> <li>■ Sign dimensions (height, width) for each and every proposed sign</li> <li>■ Sign area of each sign (total sign area, not just message area, in square feet)</li> <li>■ Sketches and elevations depicting proposed sign design, including colors &amp; materials, copy (type face, lettering size, logos, etc.), architectural embellishments, etc.</li> <li>■ Color and building materials palette(s) for all proposed signs</li> <li>■ Any additional items identified by City Staff as being important to understanding the design intent of the proposed sign(s)</li> </ul>
<b>STANDARD CENTER IDENTIFICATION SIGNS</b> (When Two or More Businesses Are Located on the Same Parcel)	
<b>Number of Signs</b>	No more than one Standard Community Service Center Identification Sign is permitted per public street frontage. Additional signs may be allowed subject to approval of a Site Development Permit by the City per Section 9-1-150 of the adopted Laguna Niguel Municipal Code.
<b>Location(s)</b>	<p>Standard Community Service Center Identification Signs shall be located for maximum visibility, usually close to or adjacent to a public street right-of-way. The proposed location(s), height(s), dimensions, and designs (e.g., colors, materials, etc.) of the Standard Retail Center Identification Sign shall be approved as part of the Site Development Permit process per Section 9-1-150 of the adopted Laguna Niguel Municipal Code.</p> <p>Signs shall be located close to the street right-of-way (at driveways), but must not block cars entering and exiting the retail center. Sightlines for the safe operation of motor vehicles must be preserved and not blocked by any signs.</p>
<b>Maximum Sign Dimensions</b>	80 square feet for entire sign structure, not just the message area. Architectural accents and embellishments are encouraged. Sign shall not exceed ten feet in height.
<b>Design and Landscaping</b>	Pole signs are prohibited. Each sign shall incorporate architectural elements and embellishments and must be constructed of at least two different building materials. No simulation motion graphics, animated signs, or electronic message boards are permitted on any signs. Each sign shall be installed within a landscaped area of not less than 100 square feet.
<b>Text (Copy)</b>	Only the shopping center name and address (if appropriate) and tenant names are permitted on Standard Community Service Identification Signs. Copy may consist of tenant names (all in one color), although each tenant may elect to use their own particular type style. Tenant logos and shopping center/project logos (in any colors) are permitted. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the architecture on-site. Multiple materials are encouraged for use on each sign (a minimum of two different materials are required). Signs shall incorporate vibrant colors that create a dynamic sense of excitement.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is discouraged.

**TABLE 4-13 Signage Standards**

<b>MONUMENT/GROUND SIGNS</b>	
<b>Number of Signs</b>	One freestanding Monument/Ground Sign is permitted for each single-tenant pad.
<b>Location(s)</b>	The Monument/Ground Sign shall be located for maximum visibility, close to the street right-of-way. However, the sign must not block cars entering or exiting the site; sightlines for the safe operation of motor vehicles must be preserved. The proposed location of the Monument/Ground Sign on-site shall be approved by the City as part of a Site Development Plan (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Size</b> (for All Signs Excepting Automobile Dealerships Monument/Ground Signage)	The primary body of each Monument/Ground Sign shall not exceed four feet in height, although architectural accents and embellishments may extend an additional foot in height for a maximum total sign height of up to five feet. The total area of each sign (including architectural accents and embellishments) shall not exceed 25 square feet.
<b>Size</b> (for Automobile Dealership Monument/Ground Signage Only)	The primary body of each Monument/Ground Sign erected for an automobile dealership shall not exceed five feet in height, although architectural accents and embellishments may extend an additional foot in height for a maximum total sign height of up to six feet. The total area of each sign (including architectural accents and embellishments) shall not exceed 40 square feet.
<b>Text (Copy)</b>	Only the tenant name, tenant logo, and the building address are permitted on each Monument/Ground Sign. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the architecture on-site.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is permitted.
<b>WALL SIGNS</b>	
<b>Number of Signs</b>	The total number of Wall Signs shall be no greater than the number of tenants in said building, with the exception that: <ul style="list-style-type: none"> <li>■ Tenants with a floor area of up to 20,000 sq. ft. may have up to two wall mounted signs with no more than one sign allowed per building face.</li> <li>■ Tenants with a floor area of greater than 20,000 sq. ft. may have up to one wall mounted sign for each building face, provided that no building shall be deemed to have more than three building faces.</li> </ul>
<b>Location(s)</b>	Signs shall be located to promote visibility, especially when viewed from the adjacent I-5 Freeway right-of-way. The location(s) of each and every proposed wall mounted sign(s) as it appears on the building(s) shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code). Signs may not project above the building cornice, parapet, or roof line.
<b>Size</b> (Tenants up to 20,000 sq. ft.)	Each tenant is permitted one wall mounted sign not to exceed 50 sq. ft. in area per sign. Signs shall not exceed five feet in height.
<b>Size</b> (Tenants 20,000 sq. ft. and greater)	The permitted size of the wall mounted signs for each tenant shall be calculated as follows: One square foot of signage shall be permitted for each linear foot of building frontage on the side of the building where the main entrance is located, provided that no sign shall exceed 60% of the building frontage length. (For example, if a building has a 100-foot-long front facade, then each permitted wall mounted sign could be up to 60 feet in length.) For buildings with multiple tenants, the building frontage shall be defined as the portion of the front facade that relates to that particular building tenant. Signs shall not exceed five feet in height.
<b>Height</b>	Signs shall not project above the roof line of the building.

<b>TABLE 4-13 Signage Standards</b>	
<b>Design</b> (Applies to All Wall Signs)	Individual channel letters are recommended for Wall Signs. Can signs are prohibited. All Wall Signs shall be built as 3-Dimensional (3-D) signs. No sign, including any light box or other structural part, shall project more than 18" from the building face.
<b>Additional Design Requirements</b> (for Freeway-Oriented Wall Signs Only)	In order to promote Wall Sign uniformity and consistency, while avoiding an appearance of confusion and clutter, all freeway oriented Wall Signs shall be designed and constructed of the same type style, same type size/letter height, same colors, and same materials. In addition, the owner(s) of each building(s) or project shall be responsible for implementing a consistent Wall Sign program that all tenants within the building(s)/project shall be required to follow.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment or contrast with those used on the wall or building to which the sign is attached. Vibrant colors are encouraged. Graphic elements and/or dimensional logos shall be incorporated into signs.
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is permitted. No can lighting is permitted.
<b>MISCELLANEOUS SIGNS</b>	
<b>Canopy/Awning Signs</b>	Canopy and awning signage is not permitted.
<b>Window and Door Signs</b>	White vinyl letters are permitted on doors only. Letters shall not exceed 3 ft in height. No lettering or logos are permitted on windows.
<b>Directional Safety, Warning, or Information Signs</b> (On Private Property Only)	Directional/Information Signs shall not exceed seven feet in height. Signs may be double sided. The entire sign (including message area and sign structure) shall not exceed 20 square feet in area. Directional/Information Signs may be lighted, if desired, but only by external lighting sources. No internal or neon lighting is permitted. The exact numbers and locations of proposed Directional Safety, Warning, or Information Signs shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Directional Safety, Warning, or Information Signs</b> (within Public Rights-of-Way Only)	Directional/Information Signs within public rights-of-way may consist of either freestanding kiosks or pole-mounted signs or banners/signs erected on light standards. All proposed public Directional Safety, Warning, or Information Signs shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>BUSINESS PARK (BP)</b>	
<b>GENERAL STANDARDS</b>	
<b>Signage Concept</b>	Signs in the Business Park (BP) district shall help establish an active, contemporary, pedestrian oriented atmosphere. Signs shall be mounted lower than normal for typical retail centers in order to promote visual excitement and activity at the pedestrian level. The signs shall be engaging and multi-layered, but tasteful in character.
<b>Colors</b>	Colorful signs are encouraged in the BP district within Laguna Niguel Gateway. Exciting color combinations with high contrast accents are especially appropriate; colors shall be clear and vibrant. Sign colors shall read well against the building facade colors.
<b>Materials and Finishes</b>	Combinations of metals, carved wood, rigid foams, meshes, perforated materials, tiles, and painted and glossy finishes are encouraged.

**TABLE 4-13 Signage Standards**

<p><b>Required Sign Review</b></p>	<p>All projects within the BP district must obtain a Site Development Permit per Section 9-1-150 of the Laguna Niguel Municipal Code to establish a project sign program. The application package for the Site Development Permit shall include, as a minimum, the following:</p> <ul style="list-style-type: none"> <li>■ A Sign Plan depicting the location of each and every proposed sign.</li> <li>■ Sign dimensions (height, width) for each and every proposed sign.</li> <li>■ Sign area of each sign (total sign area, not just message area, in square feet).</li> <li>■ Sketches and elevations depicting proposed sign design, including colors &amp; materials, copy (type face, lettering size, logos, etc.), architectural embellishments, etc.</li> <li>■ Color and building materials palette(s) for all proposed signs.</li> <li>■ Any additional items identified by City Staff as being important to understanding the design intent of the proposed sign(s).</li> </ul>
<p><b>PRIMARY PROJECT IDENTIFICATION SIGNS</b></p>	
<p><b>Number of Signs</b></p>	<p>No more than one Primary Project Identification Sign may be erected per project. A project shall be defined as any retail, support/service commercial, highway commercial, or light industrial development consisting of no less than 75,000 square feet of leasable or usable floor area, excluding covered or structured parking areas, covered loading/unloading areas, and covered trash enclosures. Signs may be double sided. Projects not qualifying for a Primary Project Identification Sign are eligible for one freestanding Monument/Ground Sign. Projects consisting of no less than 120,000 square feet of leasable or usable floor area may erect one Primary Project Identification Sign and one Monument/Ground Sign. Projects consisting of no less than 150,000 square feet of leasable or usable floor area may erect one Primary Project Identification Sign and <u>either</u> a Secondary Project Identification Sign <u>or</u> a Monument/Ground Sign (but not both).</p>
<p><b>Location(s)</b></p>	<p>Primary Project Identification Signs shall be located close to the Camino Capistrano street right-of-way, but must not block cars entering and exiting the project. Sightlines for the safe operation of motor vehicles must be preserved and not blocked by any signs.</p>
<p><b>Maximum Sign Dimensions</b></p>	<p>80 square feet for entire sign structure, not just the message area. Architectural accents and embellishments are encouraged. Sign shall not exceed ten feet in height.</p>
<p><b>Design and Landscaping</b></p>	<p>Pole signs are prohibited. Each sign shall incorporate architectural elements and embellishments and must be constructed of at least two different building materials. Each sign shall be installed within a landscaped area of no less than 120 square feet (raised planter boxes are acceptable, provided that the sign height, inclusive of the planter box height, does not exceed 15 feet in height).</p>
<p><b>Text (Copy)</b></p>	<p>Each Primary Project Identification Sign shall include a list of some or all of the major tenants located within the project, provided that no more than six tenants are permitted per sign. Tenant logos and tenant names (in any color and type style) are permitted. In addition, the name, logo, and address of the shopping center (if available) are permitted as well. Copy may appear on both sides of the sign.</p>
<p><b>Colors, Materials, and Finishes</b></p>	<p>Signage colors, materials, and finishes shall compliment those used on the architecture on-site. Multiple materials are encouraged for use on each sign (a minimum of two different materials are required). Signs may incorporate vibrant colors that create a dynamic sense of excitement.</p>
<p><b>Lighting</b></p>	<p>Primary Project Identification Signs may be internally and/or externally illuminated. Where feasible, external light sources shall be concealed from view by sign elements, landscaping, or other design features. Use of neon lighting and fiber optics is discouraged, but not prohibited.</p>

<b>TABLE 4-13 Signage Standards</b>	
<b>SECONDARY PROJECT IDENTIFICATION SIGNS</b>	
(Permitted Only When Used in Conjunction with Major Project Center Identification Signage within the Same Retail Center or Project)	
<b>Number of Signs</b>	No more than one Secondary Project Identification Sign is permitted per street frontage. A project must contain at least 150,000 square feet of leasable or usable floor area (excluding covered or structured parking areas, covered loading/unloading areas, and covered trash enclosures) in order to qualify for both Primary and Secondary Project Identifications Signs. In no case shall any project have more than two Secondary Project Identification Signs. Sign may be double sided.
<b>Location(s)</b>	Secondary Project Identification Signs shall be located close to the street right-of-way (at driveways), but must not block cars entering and exiting the retail center. Sightlines for the safe operation of motor vehicles must be preserved and not blocked by any signs.
<b>Maximum Sign Dimensions</b>	50 square feet for the entire sign structure, not just the message area.
<b>Design and Landscaping</b>	Pole signs and simulation motion signs and graphics are prohibited. Each sign shall incorporate architectural elements and embellishments and must be constructed of at least two different building materials. Each sign shall be installed within a landscaped area of not less than 85 square feet (raised planter boxes are acceptable, provided that the sign height, inclusive of the planter box height, does not exceed 10 feet).
<b>Text (Copy)</b>	The name and address of the shopping center and tenant names are permitted on Secondary Project Identification Signs. The tenant names shall be all in one color, although each tenant may elect to use their own particular type style. Tenant logos are <u>not</u> permitted, although the project logo or shopping center logo is allowed. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the Primary Project Identification Sign, as well as the architecture on-site. Multiple materials are encouraged for use on each sign (a minimum of two different materials are required). Signs shall incorporate vibrant colors that create a dynamic sense of excitement.
<b>Lighting</b>	Secondary Project Identification Signs may be illuminated by external and/or internal light sources. Where feasible, light sources shall be concealed from view by sign elements, landscaping, or other design features. Use of neon lighting and fiber optics is discouraged, but not prohibited.
<b>MONUMENT/GROUND SIGNS</b>	
<b>Number of Signs</b>	One freestanding Monument/Ground Sign is permitted for each single-tenant pad.
<b>Location(s)</b>	The Monument/Ground Sign shall be located for maximum visibility, close to the street right-of-way. However, the sign must not block cars entering or exiting the site; sightlines for the safe operation of motor vehicles must be preserved. The proposed location of the Monument/Ground Sign on-site shall be approved by the City as part of a Site Development Plan (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Size</b>	The primary body of each sign shall not exceed 4 feet in height, although architectural accents and embellishments may extend up to 5 feet in height. The total area of each sign shall not exceed 24 square feet.
<b>Text (Copy)</b>	Only the tenant name, tenant logo, and the building address are permitted on each Monument/Ground Sign. Copy may appear on both sides of the sign.
<b>Colors, Materials, and Finishes</b>	Signage colors, materials, and finishes shall compliment those used on the architecture on-site.

<b>TABLE 4-13 Signage Standards</b>	
<b>Lighting</b>	Signs may be illuminated by external and/or internal light sources. Use of exposed neon lighting and fiber optics is discouraged, but not prohibited.
<b>WALL SIGNS</b>	
<b>Number</b>	The total number of Wall Signs shall be no greater than the number of tenants in said building, with the exception that: <ul style="list-style-type: none"> <li>■ Tenants with a floor area of up to 20,000 sq. ft. may have up to two Wall Signs with no more than one Wall Sign allowed per building face.</li> <li>■ Tenants with a floor area of greater than 20,000 sq. ft. may have up to one wall mounted sign for each building face, provided that no building shall be deemed to have more than three building faces.</li> </ul>
<b>Location(s)</b>	Signs shall be located to promote visibility, especially when viewed from the adjacent I-5 Freeway right-of-way. The location(s) of each and every proposed Wall Sign(s) as it appears on the building(s) shall be approved by the City as part of a Site Development Plan (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Size</b> (Tenants up to 20,000 sq. ft.)	The permitted size of the wall mounted signs for each tenant shall not exceed 50 square feet in area. Signs shall not exceed five feet in height
<b>Size</b> (Tenants 20,000 sq. ft. and greater)	The permitted size of the wall mounted signs for each tenant shall be calculated as follows: One square foot of signage shall be permitted for each linear foot of building frontage on the side of the building where the main entrance is located, provided that no sign shall exceed 60% of the building frontage length. (For example, if a building has a 100-foot-long front facade, then each permitted wall mounted sign could be up to 60 feet in length.) For buildings with multiple tenants, the building frontage shall be defined as the portion of the front facade that relates to that particular building tenant. Signs shall not exceed five feet in height.
<b>Height</b>	Signs may not project above the building cornice, parapet, or roof line.
<b>Design</b> (Applies to All Wall Signs Regardless of Location)	Individual channel letters are recommended for all Wall Signs. Can signs are prohibited. All Wall Signs shall be built as 3-Dimensional (3-D) signs. No sign, including any light box or other structural part, shall project more than 18" from the building face.
<b>Additional Design Requirements</b> (for Freeway-Oriented Wall Signs Only)	In order to promote Wall Sign uniformity and consistency, while avoiding an appearance of confusion and clutter, all freeway oriented Wall Signs shall be designed and constructed of the same type style, same type size/letter height, same colors, and same materials. In addition, the owner(s) of each building(s) or project shall be responsible for implementing a consistent Wall Sign program that all tenants within the building(s)/project shall be required to follow.
<b>Depth of Projection</b>	No such sign, including any light box or other structural part, shall project more than 18" from the building face.
<b>Lighting</b>	Wall mounted signs may be internally and/or externally illuminated. Use of neon lighting and fiber optics is discouraged, but not prohibited. Can lighting is prohibited.
<b>BLADE SIGNS</b>	
<b>Number of Signs</b>	No more than one blade sign shall be permitted per building entry (excludes employee, service, and emergency entries).
<b>Location(s)</b>	Signs shall be located over or adjacent to the sidewalk from which the primary building entry occurs. Where appropriate, blade-type signs that are mounted perpendicular to the building shall be erected around the front entrance of stores.
<b>Size</b>	Signs shall not be larger than 4 square feet in size for the entire sign structure, not just the message area.

TABLE 4-13 Signage Standards	
<b>Text (Copy)</b>	Copy shall be limited to the tenant's name and logo only. Tenants may elect to use their own particular type and logo colors and styles. Tenant's logos may be used in addition to or in place of the text.
<b>MISCELLANEOUS SIGNS</b>	
<b>Canopy/Awning Signs</b>	Awnings and canopies are permitted, but may not be used as signage (no text or logos permitted). However, blade signs may be erected under awnings and canopies.
<b>Window and Door Signs</b>	White vinyl letters are permitted on doors only. Letters shall not exceed 3 ft in height. No lettering or logos are permitted on windows.
<b>Directional Safety, Warning, or Information Signs (On Private Property Only)</b>	Directional Safety, Warning, or Information Signs shall not exceed 7 feet in height. These signs may be double sided. The entire sign area (including message area and sign structure) shall not exceed 20 square feet. Directional, Safety, Warning Signs shall be externally lighted. No internal or neon lighting is permitted. The exact numbers and locations of proposed Directional Safety, Warning, or Information Signs shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).
<b>Directional Safety, Warning, or Information Signs (within Public Rights-of-Way Only)</b>	Directional Safety, Warning, or Information Signs within public rights-of-way may consist of either freestanding kiosks or pole-mounted signs or banners/signs erected on light standards. The exact numbers and locations of proposed public Directional Safety, Warning, or Information Signs shall be approved by the City of Laguna Niguel as part of a Site Development Permit (see Section 9-1-150 of the adopted Laguna Niguel Municipal Code).

### Sign Amortization/Abatement

This section includes specific sign amortization/abatement requirements, as well as suggested amortization procedure options that could be used as the basis for initiating and/or enticing nonconforming signs to be replaced or updated:

- A nonconforming sign that does not conform to the standards set forth in this section shall not be:
  - > Structurally altered to extend its useful life.
  - > Expanded.
  - > Re-established after damage or destruction of 50% or more of its value, as determined by the City of Laguna Niguel Building Department.
  - > No new sign shall be approved for a site, structure, building, or use that contains conforming signs unless such nonconforming signs are removed or modified to conform to the provisions of this chapter. No building permit shall be issued for any structures, building expansions, or new building construction on the site which contains nonconforming signs, unless all signs on the site are brought into conformance with this chapter. This standard shall not

apply to interior alterations which do not substantially change the character or intensity of the site or use.

- Options to encourage nonconforming signs to accelerate their amortization period could include, but are not limited to, the following:
- Financial assistance with property owners to encourage the removal of old, nonconforming signage and installing new signage that meets the intent and requirements of this chapter. For example, the City could choose to pay a portion of the total signage costs (i.e., removal of old signage and design & installation of new signage). Optional amortization programs are defined below:
  - > Within 2 years of adoption of this Specific Plan: City pays 50% of total signage costs
  - > Within 4 years of adoption of this Specific Plan: City pays 30% of total signage costs
  - > Within 5 years of adoption of this Specific Plan: City pays 10% of total signage costs
  - > Anytime after 5 years: City pays 0% of total signage costs
  - > City requires that all signs within the Laguna Niguel Gateway Specific Plan area be replaced within a specified time period (i.e., no later than six years from the date of adoption of this Specific Plan).
  - > City arranges for low interest business loans for property owners/business owners who wish to update their signage.
  - > City elects to waive or reduce some of the development and processing fees (e.g., building permit fees, rezoning fees, etc.) associated with business and property owner improvements to property within the Laguna Niguel Gateway Specific Plan area.

### **Signage Design Review**

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The Planning Commission shall be the reviewing/approving authority for all signage to be erected within the Laguna Niguel Gateway Specific Plan area. The Planning Director shall be responsible for reviewing all design proposals for individual project specific signage (e.g., freestanding/monument signs, Center Identification Signs, wall signs, blade signs, directional signage, etc.). Appeals on decisions made regarding project related signage shall be forwarded to the Planning Commission for final decision.

## 4.5 DESIGN GUIDELINES

These guidelines establish a clear set of qualitative guiding principles that provide the City of Laguna Niguel and future applicants with a basis for designing and reviewing new development projects. The guidelines serve two primary purposes:

1. To establish high-quality design for new development
2. To facilitate the design review and planning approval process for projects relative to building configuration and architecture that is appropriate for particular building types and compatible to adjacent buildings

The guidelines are not intended as a literal requirement; instead they serve as a framework for the design review process and cover key elements including building mass, entrances, façade design and composition, roof forms, materials, and colors. The City shall use the guidelines as part of the formal design review process, and qualitative findings shall be made by the decision-making body demonstrating such compliance. Table 4-14 (Design Guidelines Applicability) specifies applicability of the guidelines in this section per Planning District (Figure 4-2) and Development Standards.

Development Standards	Planning District	A,B	C	D	E		F	G	H	I,K,J
	Land Use Designation	BP	MU	MU	MU	RC	MU	MU	MU	CS
<b>Setbacks</b>		NA	Per Section 4.5.1						NA	
<b>Building Elevations</b>		NA	Per Section 4.5.2						NA	
<b>Parking</b>		Per Section 4.5.3								
<b>On-Site Open Space</b>		NA	Per Section 4.5.4						NA	
<b>Building Types</b>		NA	Per Section 4.5.5							
<b>Streetscape Guidelines</b>		NA	Per Section 4.5.6							
NA = not applicable										

### 4.5.1 Setbacks

The treatment of setback areas along pedestrian-oriented streets is essential to the creation of vibrant mixed-use pedestrian corridors. The following guidelines should be considered when designing street-front setback areas. Their applicability by planning district is specified in Table 4-14.

1. Where possible on-site open space should be publicly accessible, located along street-front setback areas, and adjacent to existing open space.
2. Front setback areas used for publicly accessible on-site open space should be designed to complement existing open space where possible so that, over time, the individual open spaces will cumulatively create a larger open space. Along Forbes Road, they should be located within visual proximity to the Oso Creek Trail.
3. Front setback areas used for outdoor dining should be located directly adjacent to the restaurant space they belong to.
4. Outdoor dining areas within front setbacks are encouraged to include planters or other landscape elements.

### 4.5.2 Building Elevations



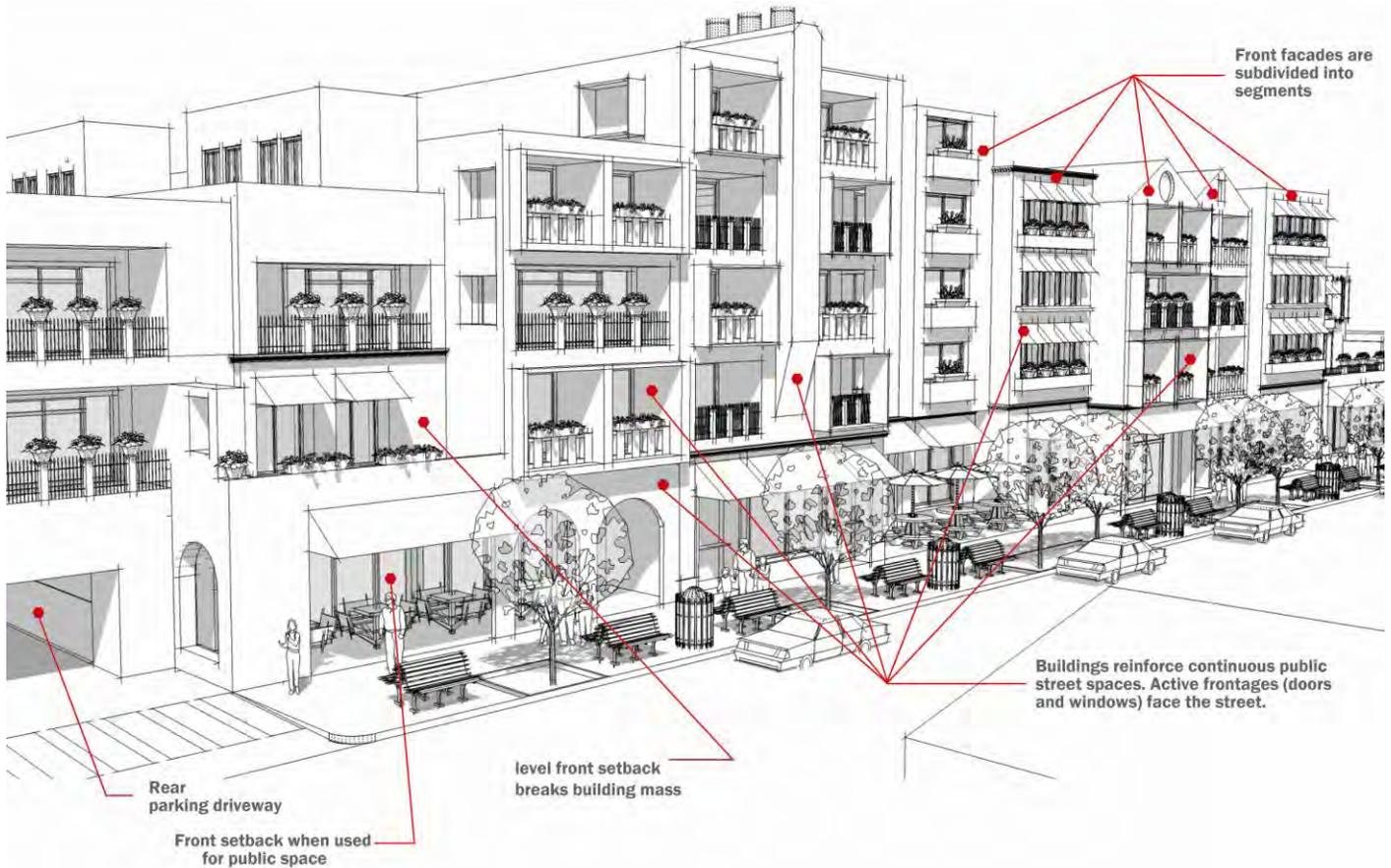
The use of different colors, materials, and other façade elements help break building mass

One of the most important architectural elements to regulate is the facades of buildings. The guidelines in this section include different techniques to regulate the different elements of a building's façade, including articulation; design and composition; entrances; roof forms, materials, and colors. These guidelines should be considered as general techniques when designing building facades along pedestrian-friendly corridors. Their applicability by planning district is specified in Table 4-14.

#### Façade Articulation

1. Street-facing building facades should provide a continuous front-street wall along pedestrian walkways and landscaped areas.
2. Buildings with first floor façades that are 50 feet or longer should be subdivided into shorter vertical segments. This may be done through the following techniques:
  - a. Façade segmentation may be accomplished through recessed façade elements. Minimum façade recession areas should be 4 feet and may be designed to include an open space area, per Section 4.4.4 (On-Site Open Space).
  - b. Changes in window/façade composition
  - c. Changes in wall materials
3. Second-floor (or higher) facades should not extend greater than 100 horizontal linear feet without some manner of vertical articulation. This may be done through the following techniques:

- a. Façade segmentation may be accomplished through recessed façade elements. Minimum allowed façade recession should be 4 feet and may be used for roof top open space areas.
- b. Changes in window/balcony/façade composition
- c. Changes in wall materials/colors



Mixed-Use building showing first and second level façade articulation and segmentation



Architectural angled corner element in retail/office mixed-use building

4. For buildings located on corners, mass articulation is recommended at the building corners, including an angled architectural feature, a pilaster, the location of a building entrance, or a corner tower element that extends from the ground floor.
5. For key intersections or gateways, prominent corner architectural features such as prominent entries or corner towers are recommended.

### Façade Design and Composition



Large Mixed-Use project with façade modulation to make building appear as a series of different buildings

1. Buildings should maintain architectural details and the same level of visual quality used on a front façade on all visible façades of buildings.
2. Large projects should be broken into buildings with a series of façade modulations that make the building appear as a series of different buildings.
3. The following guidelines should apply to building base/ground floor treatments:
  - Breaks in a building’s ground level street-front façade should be limited to those necessary to accommodate pedestrian pass-throughs, open space areas, and permitted vehicular access to driveways and drop-off areas.
  - Buildings should include a base treatment at ground floor level that establishes human scale for pedestrians.
  - Design components of base treatments may include, but are not limited to:
    - > A thicker base portion of the ground floor of a minimum height of 1 foot
    - > A material or color change
    - > A cornice line/protruding horizontal band
    - > A ground-level colonnade or arcade
    - > A ground floor minor recess, no greater than 1 foot, unless used for entry or driveway access
    - > A change in façade window articulation



Office building showing a protruding horizontal shelter base treatment element at ground level

- For buildings that are four stories or higher, the base treatment may extend up to the top of the second level



Base treatment may extend up to the top of the second level on buildings that are four stories or higher



Ground level minor recess base treatment



Thicker base portion and continuous awning element create a pedestrian-scaled ground level façade in this retail/office Mixed-Use building

4. On Mixed-Use buildings, upper-story window-to-wall ratios should be lower than the ground floor.
5. Windows on ground level should encompass:
  - Retail uses: a minimum of 60% and a maximum of 90% of the building's ground level façade
  - Office uses: a minimum of 40% and a maximum of 90% of the building's ground level façade
6. Curtain wall window treatments are permitted on second and upper levels on office uses. They are not permitted on residential uses.



Mixed-Use building with upper-story window-to-wall ratio that is lower than the ground floor

## Entrances

1. Residential and Mixed-Use Buildings (Retail/Office and Residential on top)
  - a. Main entrances to buildings should be prominent, easily identifiable, and accessible.
  - b. Each entrance should have an architectural definition element such as an awning, a trellis, a recessed niche, a 3-dimensional feature, or other shelter projection elements not exceeding 4 feet.



Easily identifiable main entrance to Multi-Family building



Easily identifiable main entrance to Mixed-Use building



Awnings help make building access points more prominent and can help create a visually attractive contrast on a building façade



Ground level trellises add shelter and create pedestrian scale along front façades

- c. Rear entrances to buildings should be visible and easily identifiable.
- d. Rear entrances should not be more prominent or taller than front primary entrances.
- e. Awnings and trellises are permitted along retail uses on ground level and residential uses on ground level or upper levels and should comply with the following:
  - Awnings should be built of permanent high-quality materials that are consistent with the overall building design.
  - Fabric awnings, when used, should be made of colored fabric over a metal structural frame. Internally illuminated fabric awnings are not permitted.
  - Forms of trellises, awnings, and/or canopies over entrances should be derived from the overall architectural style of the building.
  - Awnings, trellises, and canopies should be a minimum of 8 feet above grade.

## 2. Office and Retail Buildings

- a. Main entrances to buildings should be prominent, easily identifiable, and accessible.
- b. Main entrances should have an architectural definition element such as a trellis, a recessed niche, a 3-dimensional feature or other shelter projection elements not exceeding 4 feet.

- c. Awnings are only permitted on retail uses at ground level and trellises and other types of shelter projection elements are recommended for office uses, and should comply with the following:
- Awnings should be built of permanent good quality materials that are consistent with the overall building design.
  - Fabric awnings, when used, should be made of colored fabric over a metal structural frame. Internally illuminated fabric awnings are not permitted.
  - Awnings and/or canopies over entrances should be derived from the overall architectural style of the building.
  - Awnings, trellises, and canopies should be a minimum of 8 feet above grade.



Contemporary style office building with precast concrete as a base treatment on ground level

## Roof Forms

1. Variation of roof forms, heights, and profiles is encouraged.
2. Roofs should match the building in terms of style, detailing, and materials.
3. Roof overhangs should be compatible with the architectural style.
4. Recommended roof materials include metal seam roofing, corrugated metal roofing, terra cotta or concrete tile, and tar and gravel (for flat roofs which are not visible from the street).
5. Roof mounted equipment should be screened by architectural enclosures that relate to the building's overall architectural style.

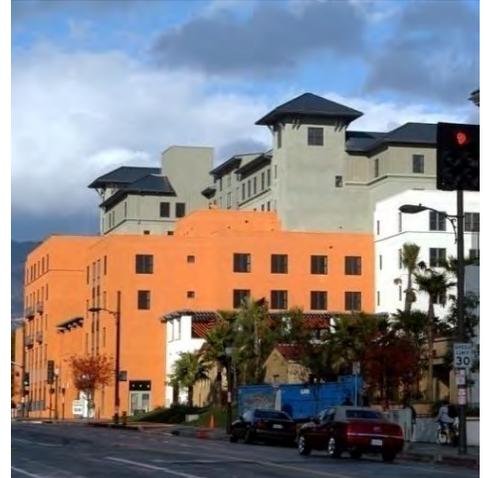


Office complex with varying roof heights allowing for rooftop open space terraces

6. Roof drainage components should be incorporated into the overall architectural composition of the façade and roof.



Varying building roof heights provide opportunities for roof terraces and help break building mass



Varying building heights and profiles help break building mass and create a more visually attractive building

## Materials

1. Sign materials should relate to and unify building appearance and also allow for expression of individual building segments.
2. The palette of wall materials per building should be minimized; preferably two or less.
3. For wood siding, painted wood is recommended.
4. Stucco, cement plaster, or stucco like finishes are acceptable, provided the stucco finish is smooth, such as a smooth trowel or fine sand float finish, or dash, rather than textured, lace, or rough sand finish.



Illustrative image of not permitted rough sand finish stucco



Illustrative image of permitted smooth stucco finish

- Ceramic tile is recommended as a decorative wall accent material when compatible with building style.
- Stone and stone veneer is not recommended as the primary building cladding material; however, it is recommended as an accenting material.



Illustrative examples of permitted stone accent materials

- Brick is not recommended as the primary building cladding material; however, it is recommended as an accenting material. No more than one brick type or tone is permitted per project. Permitted brick types and tones include, but are not limited to:



Illustrative examples of permitted brick types

- Precast concrete, poured-in-place concrete or concrete block is permitted as a primary and/or secondary material when compatible with building style.
- Corrugated or rolled metal is not recommended as a primary building cladding material; however, it is recommended as an accenting material.

## Colors

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- Colors should create visual façade articulation and/or accentuate architectural details of a building and be consistent with its style.

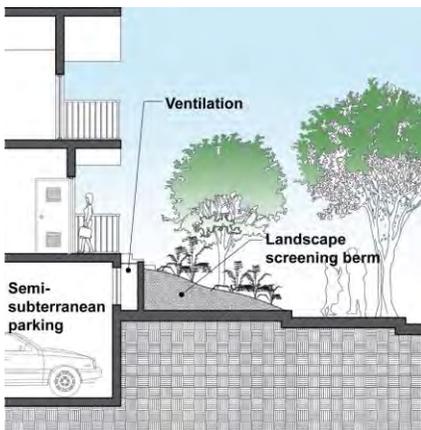
2. No more than three different colors should be used on all façades of one particular building.
3. Color for trim, awnings, and other highlights should accent and contrast with wall colors.
4. Use of bright and dark colors is only recommended for façade details and for creating contrast with the main building mass.
5. Sign colors should relate or contrast with the building wall color where the sign is mounted. Refer to Section 4.4.7 (Signage Standards by Land Use) for building signage standards.



Bright colors may be used to create contrast, façade articulation, and accentuate architectural details of a building such as an entrance.

### 4.5.3 Parking

Parking supply, configuration, placement, and access will be essential to the function and vitality of the Specific Plan area. Parking areas which are visible from main street fronts lead to a loss of vitality and street life. The following guidelines should be considered when designing parking facilities. Their applicability by planning district is specified in Table 4-14.



Berms or other landscape treatments should be used to screen semisubterranean parking garages

- Structured, surface, and semi-subterranean parking areas should be designed and placed to be unobtrusive and have minimal impact on the character and scale of commercial, mixed-use, and residential streets. Figure 4-3 shows all flexible setback areas where surface and structure parking is recommended for properties along build-to lines; in most cases on the rear of the building or away from the main pedestrian-oriented street front.
  - > Subterranean parking may extend to all property lines.
  - > Semi-subterranean parking in residential buildings may extend to all property lines, provided that the aboveground section of the semi-subterranean parking garage does not

rise up more than 4 feet from ground level and is completely screened from street view by landscape treatments.

- > Recommended parking access points along build-to lines are illustrated in Figure 4-3. These access points are meant to illustrate approximate parking access locations.
- > Trash enclosures should be designed in a manner that is consistent with the main building's architecture.
- > If a parking structure is well designed and integrated to other uses, it does not need to be screened by dense landscaping in an urban setting. However, where the City's Reviewing Agency determines that conformance with this Code is not feasible, a parking structure may be screened with landscaping using columnar trees in a minimum 8-foot-wide setback and staggered with the street trees. In combination, the setback and street trees should screen the parking structure from view.

■ Drop-off zones

- > Drop-off zones should be located along the curb or within parking facilities to promote sidewalk/street wall continuity and reduce conflicts with pedestrians.

#### 4.5.4 On-Site Open Space

Provision of on-site Open Space is essential to the creation of a green and pedestrian friendly network throughout the Specific Plan area. The following guidelines encourage the creation of accessible and appropriately designed open spaces. Their applicability by planning district is specified in Table 4-14.

- Open spaces should be designed to have the character of outdoor nonroofed areas contained by building walls.
- Plazas and courtyards should incorporate amenities beyond the minimum required, including permanent and/or temporary seating, to facilitate their enjoyment and use. Seating should be placed with consideration to noontime sun and shade; deciduous trees should be planted as the most effective means of providing comfortable access to sun and shade.
- Landscape elements should support an easy transition between indoors and outdoors through such means as well-sited and comfortable steps, shading devices and/or planters that mark building entrances, etc.



Streetscape can complement a well-designed parking structure, particularly in conjunction with an active ground floor



Photovoltaic panels may be incorporated into the roof of parking structures or over surface parking



In limited circumstances, a green screen (above) or dense tree planting (below) can screen an unimproved concrete structure



Open space and landscaping can take a variety of forms



Open space and streets should be designed to accommodate a variety of activities and events

- Landscape elements should establish scale and reinforce continuity between indoors and outdoors space. Canopy trees planted in minimum 36-inch boxes that will achieve a height of at least 35 feet in 10 years shall be provided within open spaces, especially along streets and required setbacks.
- Landscape elements should provide scale, texture, and color. A rich, coordinated palette of landscape elements that enhances the development site’s identity is encouraged.
- Landscaping should be used to screen or break up the mass of blank walls. Trees and shrubs may be planted in front of a blank wall where there is room or vines may be trained on the wall where space is limited.

### 4.5.5 Building Types

The following guidelines provide building type design recommendations, including: access, parking, on-site open space, massing, and frontage type. Their applicability by planning district is specified in Table 4-14. General guidelines, which apply to all building types, are listed first, followed by building-type-specific guidelines (refer to Section 4.4.5 (Building Types) for Building Type configuration and standards).

#### A. BUILDING TYPE GENERAL GUIDELINES

##### Access



Center courtyard area of a mixed-use building

1. Main entrances to buildings should be from the front street(s).
2. Where an alley is present, parking and services should be accessed through the alley.
3. For mixed use, non-residential uses should be located at ground level and oriented to the street. Individual street front access to retail/commercial uses should be provided for each space. A portion of the ground level street-oriented space in mixed-use buildings may also be used for residential units if applicable to the building type.
4. For multi-family buildings, front-street façades should be lined with dwelling units oriented to the street at ground level. Individual direct street access to ground level units should also be provided when possible.



Individual front street dwelling entrances promote street life and help break building mass

## **Parking**

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1. Parking should be located in a subterranean garage, or may be surface parking, semi-subterranean parking, aboveground level parking garage, or a combination of any of the above.
2. Parking access and/or driveways should be located as close as possible to the side or rear of each lot.
3. Surface parking, aboveground level and semi-subterranean parking garage areas should be screened from street view with retail/office uses or dwelling units and landscaping techniques.
4. On corner lots, when parking restrictions don't allow more than one façade of the building to be lined with retail/commercial or residential units at ground level. Surface parking, aboveground level, or semi-subterranean parking garage areas should be screened with landscape along the secondary street front.
5. All façades fronting Forbes Road should screen parking areas with retail/commercial uses or residential units depending on the land use designation allowance.
6. Rear surface parking areas should be screened from the street with landscape and a decorative wall.

## **On-Site Open Space**

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1. Private patios may be provided at side yards, front yards, rear yards, and part of courtyards.
2. Private patios along street fronts may have enclosures up to 4 feet high for privacy.
3. Courtyards located over parking garages should be designed to include landscaping elements including tree planting enclosures and planters.

## **Building Massing**

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1. Three-story or higher buildings should be designed to locate the lower building masses to the south of the site to allow for maximum exposure of sunlight to open space areas and dwelling units.
2. Each individual dwelling unit should have at least one side exposed to the outdoors with direct view to the street, a courtyard, a patio, or a combination of the above.

## Frontage Types

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### 1. Residential Buildings

- a. Front porch and stoop frontages are recommended (refer to Figure 4-12 [Street Frontages—Front Porch] and Figure 4-13 [Street Frontages—Stoop]) and may be arranged along a front setback, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4).
- b. Frontage types should provide a transition from public to private, indoor to outdoor at the main entrance to multi-family buildings as well as at all direct street entrances to individual dwelling units (if any).
- c. A front porch or stoop may be substituted by a private patio or a landscaped area when no individual direct street access to ground-level units is provided.

### 2. Mixed-Use: Residential over Retail/Office

- a. Shopfront, gallery, and arcade frontages are recommended for all ground-level retail uses (refer to Figure 4-14 [Street Frontages—Shopfront], Figure 4-15 [Street Frontages—Gallery], and Figure 4-16 [Street Frontages—Arcade]) and may be arranged along a front setback, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4).
- b. Glass front, gallery and arcade office frontages are recommended for all ground-level office uses (refer to Figure 4-17 [Street Frontages—Office]) and may be arranged along a front setback (when allowed), or front street façade, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4)
- c. Front porch and stoop frontages are recommended for all residential portions of Mixed-Use buildings at ground-level (refer to Figure 4-12 [Street Frontages—Front Porch] and Figure 4-13 [Street Frontages—Stoop]) and may be arranged along a front setback (when allowed), or a front street façade, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4).
- d. A front porch or stoop may be substituted by a private patio or a landscaped area when no individual direct street access to ground-level units is provided.

**3. Mixed-Use: Office and Retail**

- a. Shopfront, gallery, and arcade frontages are recommended for all ground-level retail uses (refer to Figure 4-14 [Street Frontages—Shopfront], Figure 4-15 [Street Frontages—Gallery], and Figure 4-16 [Street Frontages—Arcade]) and may be arranged along a front setback, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4).
- b. Glass front, gallery and arcade office frontages are recommended for all ground-level office uses (refer to Figure 4-17 [Street Frontages—Office]) and may be arranged along a front setback (when allowed), or front street façade, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4)

**4. Office Buildings**

- a. Glass front, gallery and arcade office frontages are recommended (refer to Figure 4-17 [Street Frontages—Office]) and may be arranged along a front setback (when allowed), or front street façade, a paseo, an entry forecourt, a plaza, or a corner plaza open space (refer to Section 4.4.4)

**5. Hotel**

- a. No specific requirements apply to street frontages for hotel uses



A front porch is a frontage wherein the façade is set back from the frontage line with an attached roofed porch that extends into the sidewalk or front open space area. Porches should have a minimum dimension of 8 feet wide by 6 feet deep and the roof area of porches may be used as roof terrace space. This type is used for residential uses with individual unit street access. A front porch may be raised from ground level up to 4 feet. Front porches may be placed above semi-subterranean parking garages, provided that the aboveground section of the semi-subterranean parking garage is screened from street view with landscape elements.

**FIGURE 4-12 Street Frontages—Front Porch**



A stoop is a frontage wherein the façade is aligned close to the frontage line with the first story elevated from the sidewalk to add privacy for the windows. The entrance is usually an exterior stair and landing. This type is used for residential uses with individual unit street access. A stoop should be raised from ground level at least 1 foot but no more than 4 feet. Stoop landing areas may be placed above semi-subterranean parking garages, provided that the aboveground section of the semi-subterranean parking garage is screened from street view with landscape elements.

**FIGURE 4-13 Street Frontages—Stoop**



A shopfront is a frontage wherein the façade is aligned close to the frontage line with the building entrance at sidewalk grade. This frontage is conventional for retail use with or without housing on top. It has a substantial nonreflective glazing at the sidewalk level and should include front façade details framing the main entrance and/or awnings at main entrances that may overlap the sidewalk consistent with the City’s overhead sidewalk encroachment provisions.

FIGURE 4-14 Street Frontages—Shopfront



A gallery is a frontage wherein the façade is aligned close to the frontage line with an attached front cantilever shed or a lightweight colonnade which may overlap front open space areas. This type of frontage is convenient for retail use with or without housing on top. The gallery should be no less than 10 feet deep, which may be counted as part of the front setback and may overlap adjacent open space areas up to within 4 feet of the parkway or sidewalk area.

FIGURE 4-15 Street Frontages—Gallery



An arcade is a frontage wherein the façade may overhang the sidewalk at the frontage line. This frontage type is conventional for retail use. The arcade should be no less than 12 feet deep, which may be counted as part of the front setback and may not overhang the sidewalk more than 2 feet.

FIGURE 4-16 Street Frontages—Arcade



Office glass street frontage treatment



Office gallery street frontage treatment



Office arcade street frontage treatment

FIGURE 4-17 Office Street Frontages



Retail shop street frontage treatment

Retail gallery and arcade street frontage treatments

**FIGURE 4-18 Retail Street Frontages**

## **B. BUILDING TYPE GUIDELINES BY BUILDING TYPE**

### **COURTYARD PODIUM**

#### **On-Site Open Space**

1. Courtyard Podium buildings shall be designed to provide a central courtyard and/or partial, multiple, separated or interconnected courtyards. Refer to Section 4.4.4 for on-site open space area requirements.
2. In a project with multiple courtyards, at least two of the courtyards should conform to the patterns below.
3. Minimum courtyard dimension should be 40 feet when the long axis of the courtyard is oriented east/west and 30 feet when the courtyard is oriented north/south.
4. Courtyards shall be connected to each other and to the public way by pedestrian paseos.
5. Entrance doors, living space (living rooms and dining rooms) should be oriented toward the courtyard(s) on-site open space and/or the street whenever possible. Service rooms should be oriented to the side yards, service yards, and rear yards whenever possible.

#### **Building Massing**

1. Four-story or higher buildings masses should be minimized inside courtyards and more apparent on street frontages.
2. Accessory (detached) buildings are not recommended.

## URBAN BLOCK

### Access



Urban Block multi-family building with private yard areas along the street

1. For multi-family buildings, front street façades should be lined with dwelling units and/or lobby/entrance areas at ground level. Individual direct street access to ground level units may or may not be provided.

### Parking

1. Parking may be provided directly at the rear of any ground level front street retail/office uses. Or up to three levels of above-ground parking garage space may be provided on top of ground level retail/office uses and under residential and/or office space as shown below.

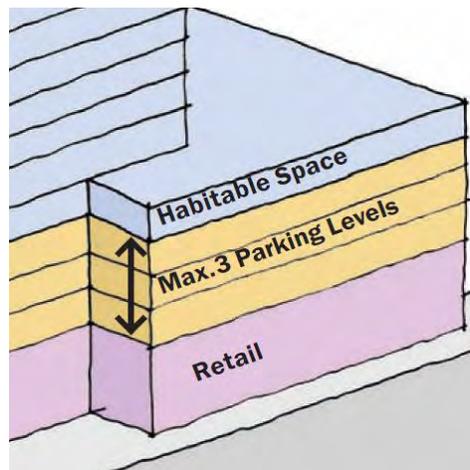


Diagram showing a street wall along a public right-of-way with ground floor retail and the maximum three parking levels with habitable space above

### On-Site Open Space

1. Urban Block buildings should be designed to provide common open space areas in the rear, side, front, and/or roof in the form of roof terraces. Refer to Section 4.4.4 for on-site open space area requirements.

### Building Massing

1. Accessory (detached) buildings are not recommended.

## LINER BLOCK

### Access

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1. Secondary building entrances should be provided to the rear of the buildings for access to parking garage structures.

### On-Site Open Space

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1. Liner Block buildings shall be designed to provide open space in the shape of courtyards, side yards, front yards, roof top terraces, or a combination of any of the above. Refer to Section 4.4.4 for on-site open space area requirements.
2. Minimum courtyard dimension should be 20 feet when the long axis of the courtyard is oriented east/west and 15 feet for a north/south orientation. Courtyards should not be of a proportion of less than 1:1 between the building height and width.

### Building Massing

---

1. Accessory (detached) buildings are not recommended.

## WRAPPED BLOCK

### Access

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1. Parking and services should be accessed through the alley when an alley is present, but due to the size of wrapped block buildings, a private service road may also be provided for parking and services access.

### Parking

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1. Parking should typically be located in aboveground parking garages that are detached to the main building structure(s) and screened from public street view.

### On-Site Open Space

---

1. Wrapped Block buildings shall be designed to provide open space in the shape of courtyards, side yards, front yards, rear yards, rooftop terraces, or a combination of any of the above. Refer to Section 4.4.4 for on-site open space area requirements.
2. Minimum courtyard dimension shall be 40 feet when the long axis of the courtyard is oriented east/west and 30 feet when the courtyard is oriented north/south.



Multi-family wrapped Block building façade hides parking structure



Wrapped Block building structure with double loaded buildings arranged around a central parking structure and courtyard open spaces

3. Courtyards should be connected to each other and to the public way by pedestrian paseos.
4. Courtyards should not be located over parking garages.

#### **4.5.6 Streetscape Design Guidelines**

The guidelines in this section address recommendations with regards to the type of plants that should be used along parkways and setback areas. Their applicability by planning district is specified in Table 4-14.

##### **Use appropriate plant materials to reinforce the character of the Specific Plan area.**

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1. Shrubs and groundcover along the primarily through streets should be drought-tolerant and low-maintenance. The use of conventional turf shall be avoided.
2. Examples of native and other drought-tolerant species and cultivars appropriate for parkways and setbacks are included in Table 4-15 (Example Species and Cultivars Suitable for Parkway and Setbacks) and corresponding Figure 4-19a through Figure 4-19e (Example Species and Cultivars Suitable for Parkway and Setbacks).

##### **Provide maintenance appropriate to native and drought-tolerant landscaping.**

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1. It is recommended that a maintenance manual/program that is appropriate for drought-tolerant plant materials is adopted and implemented for each project. In particular, it should limit irrigation to the minimum required to maintain the landscape and employ best management practices for pest control that limit the need for chemical fertilizers and pesticides unnecessary.

TABLE 4-15 Example Species and Cultivars Suitable for Parkways and Setbacks				
Botanical Name	Common Name	Water Use	Height x Spacing	Notes
<b>A. EXAMPLE TYPE 1 WALKABLE PLANTS—NO PATH REQUIRED</b>				
<b>Low Water Use/Low or No Mow Turf or Grass-like Perennials</b>				
<i>Buchloe dactyloides</i> UC Verde™	UC Verde™ Buffalo Grass	N, L	6" x 6"	Winter dormant (brown)
<i>Bouteloua gracilis</i> 'Hachita'	'Hachita' Blue Grama Grass	N, L	6" x 6"	
<i>Carex pansa</i> ( <i>C. praegracilis</i> )	California Meadow Sedge	N, M	6" x 9"+	Grows in shade or sun
<b>Low-Growing Perennials (12 inches or less)</b>				
<i>Achillea millifolium</i> cultivars	Achillea cultivars	L	12" x 3'	Mow 3 to 4x/year
<i>Chamaemelum nobile</i>	Chamomile	M	8" x 12"	
<i>Dymondia margaretae</i>	Dymondia	L	3" x 6"	Slow growing
<b>Low Water Use/Low or No Mow Turf or Grass-like Perennials</b>				
<i>Buchloe dactyloides</i> UC Verde™	UC Verde™ Buffalo Grass	N, L	6" x 6"	Winter dormant (brown)
<i>Bouteloua gracilis</i> 'Hachita'	'Hachita' Blue Grama Grass	N, L	6" x 6"	
<i>Carex pansa</i> ( <i>C. praegracilis</i> )	California Meadow Sedge	N, M	6" x 9"+	Grows in shade or sun
<b>Low-Growing Perennials (12 inches or less)</b>				
<i>Achillea millifolium</i> cultivars	Achillea cultivars	L	12" x 3'	Mow 3 to 4x/year
<i>Chamaemelum nobile</i>	Chamomile	M	8" x 12"	
<i>Dymondia margaretae</i>	Dymondia	L	3" x 6"	Slow growing
<b>Low-Growing Grasses or Grass-like Perennials (18 inches or less)</b>				
<i>Carex divulsa</i> ( <i>C. tumicola</i> )	Berkeley Sedge	N, M	12" x 2'	
<i>Festuca glauca</i> 'Siskiyou Blue' and other var.	Blue Fescue	M	12" x 12"	
<i>Pennisetum alopecuroides</i> 'Little Bunny'	Little Bunny Fountain Grass	L	12" x 12"	
<i>Sesleria autumnalis</i>	Autumn Moor Grass	M	15" x 2'	
<b>Low-Growing Perennials/Succulents (18 inches or less)</b>				
<i>Achillea millifolium</i> 'Terra Cotta'	yarrow Terra Cotta & other cultivars	L	12" x 4'	Mow 1/year for meadow
<i>Aptenia cordifolia</i> / <i>A. cordifolia</i> 'Red Apple'	Heartleaf Ice Plant	L	6" x 12"	
<i>Delosperma cooperi</i>	Trailing Ice Plant	L	8" x 15"	
<i>Drosanthemum floribundum</i>	Rosea Ice Plant	L	8" x 15"	

**TABLE 4-15 Example Species and Cultivars Suitable for Parkways and Setbacks**

Botanical Name	Common Name	Water Use	Height x Spacing	Notes
<i>Dudleya hassei</i>	Santa Catalina Live Forever	N, VL	8" x 18"	
<i>Erigeron karvinskianus</i> and <i>E. glaucus</i>	Santa Barbara & Seaside Daisy	N, M	12" x 2'	
<i>Fragaria vesca</i> ssp. <i>Californica</i> or <i>F. chiloensis</i>	Woodland or Coastal Strawberry	N, M	8" x 2'	Grows in shade
<i>Gazania rigens leucolaena</i>	Gazania (grayish leaves)	M	6" x 2'	
<i>Gazania linearis</i> 'Colorado Gold'	Colorado Gold Gazania (green leaves)	M	6" x 2'	
<i>Hypericum calycinum</i>	Creeping St. Johnswort	M	12" x 12"	Clip yearly; likes shade
<i>Iris douglasiana</i> and 'Pacific Coast Hybrids'	Douglas & Pacific Coast Iris	N, M	12" x 18"	Mix with grasses
<i>Lantana</i> Patriot series cultivars	Dwarf Lantana	L	12" x	
<i>Lessingia filaginifolia</i> 'Silver Carpet'	Beach Aster	L	12" x 4'	
<i>Monardella villosa</i>	Coyote Mint	N, VL	15" x 2'	
<i>Nepeta mussinii</i> ( <i>N. faassenii</i> )	Catmint	M	15" x 18"	
<i>Osteospermum fruitcosum</i>	Trailing African Daisy	L	6" x 18"	
<i>Oenothera caespitosa</i> and other species	Tufted evening primrose	N, L	12" x 2'	
<i>Rosmarinus officinalis</i> 'Huntington Carpet' or other prostrate varieties	Prostrate Rosemary	L	18" x 2'	
<i>Scaevola aemula</i> varieties	Fairy Fan Flower		8" x 2'+	
<i>Senecio serpens</i> , <i>S. mandraealiscae</i>	no common name	L	12" x 2'	
<i>Thymus</i> species	Thyme	M	8" x 2'	
<i>Verbena peruviana</i> and hybrids	Verbena	L	6" x 2'	
<i>Vinca minor</i>	Dwarf Periwinkle	M	12" x 4'	Plant in shade
<b>Low-Growing Shrubs (18 inches or less)—all require regular trimming at parkway edges</b>				
<i>Ceanothus</i> 'Centennial'		N, L	18" x 4'	Needs good drainage
<i>Cotoneaster dammeri</i> 'Lowfast', <i>C. salicifolia</i> 'Repens', <i>C. apiculatus</i> 'Tom Thumb'	Groundcover Cotoneaster varieties	M	18" x 4'	
<i>Juniperus horizontalis</i> and <i>J. procumbens</i> var.	Groundcover Juniper varieties	L	6-18" x 4'	See Sunset for list
<b>B. EXAMPLE TYPE 2 LOW-GROWING, LOW-MAINTENANCE PLANTS—PATH REQUIRED</b>				
<b>Low-Growing Grasses or Grass-like Perennials (18 inches or less)</b>				
<i>Carex divulsa</i> ( <i>C. tumincola</i> )	Berkeley Sedge	N, M	12" x 2'	

TABLE 4-15 Example Species and Cultivars Suitable for Parkways and Setbacks				
Botanical Name	Common Name	Water Use	Height x Spacing	Notes
<i>Festuca glauca</i> 'Siskiyou Blue' and other var.	Blue Fescue	M	12" x 12"	
<i>Pennisetum alopecuroides</i> 'Little Bunny'	Little Bunny Fountain Grass	L	12" x 12"	
<i>Sesleria autumnalis</i>	Autumn Moor Grass	M	15" x 2'	
<b>Low-Growing Perennials/Succulents (18 inches or less)</b>				
<i>Achillea millifolium</i> 'Terra Cotta'	Yarrow Terra Cotta & other cultivars	L	12" x 4'	Mow 1/year for meadow
<i>Aptenia cordifolia</i> /A. cordifolia 'Red Apple'	Heartleaf Ice Plant	L	6" x 12"	
<i>Delosperma cooperi</i>	Trailing Ice Plant	L	8" x 15"	
<i>Drosanthemum floribundum</i>	Rosea Ice Plant	L	8" x 15"	
<i>Dudleya hassei</i>	Santa Catalina Live Forever	N, VL	8" x 18"	
<i>Erigeron karvinskianus</i> and <i>E. glaucus</i>	Santa Barbara & Seaside Daisy	N, M	12" x 2'	
<i>Fragaria vesca</i> ssp. <i>Californica</i> or <i>F. chiloensis</i>	Woodland or Coastal Strawberry	N, M	8" x 2'	Grows in shade
<i>Gazania rigens leucolaena</i>	Gazania (grayish leaves)	M	6" x 2'	
<i>Gazania linearis</i> 'Colorado Gold'	Colorado Gold Gazania (green leaf)	M	6" x 2'	
<i>Hypericum calycinum</i>	Creeping St. Johnswort	M	12" x 12"	Clip yearly; likes shade
<i>Iris douglasiana</i> and 'Pacific Coast Hybrids'	Douglas & Pacific Coast Iris	N, M	12" x 18"	Mix with grasses
<i>Lantana</i> Patriot series cultivars	Dwarf Lantana	L	12" x	
<i>Lessingia filaginifolia</i> 'Silver Carpet'	Beach Aster	L	12" x 4'	
<i>Monardella villosa</i>	Coyote Mint	N, VL	15" x 2'	
<i>Nepeta mussinii</i> ( <i>N. faassenii</i> )	Catmint	M	15" x 18"	
<i>Osteospermum fruitcosum</i>	Trailing African Daisy	L	6" x 18"	
<i>Oenothera caespitosa</i> and other species	Tufted evening primrose	N, L	12" x 2'	
<i>Rosmarinus officinalis</i> 'Huntington Carpet' or other prostrate varieties	Prostrate Rosemary	L	18" x 2'	
<i>Scaevola aemula</i> varieties	Fairy Fan Flower		8" x 2'+	
<i>Senecio serpens</i> , <i>S. mandraealiscae</i>	no common name	L	12" x 2'	
<i>Thymus</i> species	Thyme	M	8" x 2'	
<i>Verbena peruviana</i> and hybrids	Verbena	L	6" x 2'	
<i>Vinca minor</i>	Dwarf Periwinkle	M	12" x 4'	Plant in shade

TABLE 4-15 Example Species and Cultivars Suitable for Parkways and Setbacks

Botanical Name	Common Name	Water Use	Height x Spacing	Notes
<b>Low-Growing Shrubs (18 inches or less)—all require regular trimming at parkway edges</b>				
<i>Ceanothus</i> 'Centennial'		N, L	18" x 4'	Needs good drainage
<i>Cotoneaster dammeri</i> 'Lowfast', <i>C. salicifolia</i> 'Repens', <i>C. apiculatus</i> 'Tom Thumb'	Groundcover Cotoneaster varieties	M	18" x 4'	
<i>Juniperus horizontalis</i> and <i>J. procumbens</i> var.	Groundcover Juniper varieties	L	6-18" x 4'	See Sunset for list
<b>C. EXAMPLE TYPE 3 MEDIUM-HEIGHT, DROUGHT-TOLERANT PLANTS—PATH AND MORE MAINTENANCE REQUIRED</b>				
<b>18" to 36" Tall Grasses</b>				
<i>Helictotrichon sempervirens</i>	Blue Oat Grass	L	2' x 2'	
<i>Leymus condensatus</i> 'Canyon Prince'	Canyon Prince Wild Rye	N, L	2' x 3'	
<i>Nasella tenuissima</i> ( <i>Stipa tenuissima</i> )	Mexican Feather Grass	N, VL	2' x 2'	
<i>Pennisetum orientale</i>	Oriental Fountain Grass	L	18" x 18"	
<i>Pennisetum setaceum</i> 'Eaton Canyon'	Dwarf Red Fountain Grass	L	2' x 3'	
<b>18" to 36" Tall Perennials/Succulents</b>				
<i>Aloe</i> 'Blue Elf' and other small varieties	Blue Elf Aloe	L	18" x 18"	
<i>Anigozanthos</i> 'Bush Pearl,' 'Bush Ranger,' and 'Bush Devil'	Kangaroo Paws varieties		2' x 2'	
<i>Limonium perezii</i>	Statice	L	2' x 3'	+ flower height
<i>Lomandra longifolia</i> 'Breeze' and 'Little Con'	Lomandra cultivars	M	2' x 3'	
<i>Penstemon heterophyllus</i> 'Margarita BOP'	Foothill Penstemon	N, M	18" x 18"	
<i>Phormium</i> 'Tom Thumb' and 'Jack Spratt'	Small Flax hybrids	M	2' x 2'	
<b>18" to 36" Tall Shrubs</b>				
<i>Arctostaphylos densiflora</i> 'Pacific Mist'		N, L	2' x 6'	
<i>Artemisia pycnocephala</i> 'David's Choice'	David's Choice Sandhill Sagebrush	N,	2' x 3'	
<i>Ceanothus gloriosus</i> 'Anchor Bay'		N, L	2' x 6'	
<i>Cistus salvifolius</i>	Sageleaf Rockrose	L	2' x 3'	
<i>Iva hayesiana</i>	Poverty Weed	N, VL	2' x 3'	
<i>Lantana montevidensis</i>	Trailing Lantana	L	2' x 3'	Cut back yearly
<i>Lantana</i> 'Gold Rush,' 'New Gold,' and 'Chapel Hill Yellow'			2' x 3'	Monrovia
<i>Mimulus</i> hybrids inc. 'Jelly Bean yellow'	Shrubby Monkeyflower hybrids	N, L	2' x 3'	
<i>Rosa</i> Flower Carpet varieties	Groundcover Roses	M	2' x 3'	Monrovia

TABLE 4-15 Example Species and Cultivars Suitable for Parkways and Setbacks				
Botanical Name	Common Name	Water Use	Height x Spacing	Notes
<i>Salvia apiana</i>	White Sage	N, VL	3' x 4'	
<i>Salvia</i> 'Bee's Bliss'	Bee's Bliss Sage	N, L	2' x 4'	
<i>Verbena lilacina</i> and <i>V. lilacina</i> 'De La Mina'	Lilac Verbena	N, L	3' x 3'	
<b>18" to 36" Tall Grasses</b>				
<i>Helictotrichon sempervirens</i>	Blue Oat Grass	L	2' x 2'	
<i>Leymus condensatus</i> 'Canyon Prince'	Canyon Prince Wild Rye	N, L	2' x 3'	
<i>Nasella tenuissima</i> ( <i>Stipa tenuissima</i> )	Mexican Feather Grass	N, VL	2' x 2'	
<i>Pennisetum orientale</i>	Oriental Fountain Grass	L	18" x 18"	
<i>Pennisetum setaceum</i> 'Eaton Canyon'	Dwarf Red Fountain Grass	L	2' x 3'	
<b>18" to 36" Tall Perennials/Succulents</b>				
<i>Aloe</i> 'Blue Elf' and other small varieties	Blue Elf Aloe	L	18" x 18"	
<i>Anigozanthos</i> 'Bush Pearl,' 'Bush Ranger,' and 'Bush Devil'	Kangaroo Paws varieties		2' x 2'	
<i>Limonium perezii</i>	Statice	L	2' x 3'	+ flower height
<i>Lomondra longifolia</i> 'Breeze' and 'Little Con'	Lomondra cultivars	M	2' x 3'	
<i>Penstemon heterophyllus</i> 'Margarita BOP'	Foothill Penstemon	N, M	18" x 18"	
<i>Phormium</i> 'Tom Thumb' and 'Jack Spratt'	Small Flax hybrids	M	2' x 2'	
<b>18" to 36" Tall Shrubs</b>				
<i>Arctostaphylos densiflora</i> 'Pacific Mist'		N, L	2' x 6'	
<i>Artemisia pycnocephala</i> 'David's Choice'	David's Choice Sandhill Sagebrush	N,	2' x 3'	
<i>Ceanothus gloriosus</i> 'Anchor Bay'		N, L	2' x 6'	
<i>Cistus salvifolius</i>	Sageleaf Rockrose	L	2' x 3'	
<i>Iva hayesiana</i>	Poverty Weed	N, VL	2' x 3'	
<i>Lantana montevidensis</i>	Trailing Lantana	L	2' x 3'	Cut back yearly
<i>Lantana</i> 'Gold Rush,' 'New Gold,' and 'Chapel Hill Yellow'			2' x 3'	Monrovia
<i>Mimulus</i> hybrids inc. 'Jelly Bean yellow'	Shrubby Monkeyflower hybrids	N, L	2' x 3'	
<i>Rosa</i> Flower Carpet varieties	Groundcover Roses	M	2' x 3'	Monrovia
<i>Salvia apiana</i>	White Sage	N, VL	3' x 4'	
<i>Salvia</i> 'Bee's Bliss'	Bee's Bliss Sage	N, L	2' x 4'	
<i>Verbena lilacina</i> and <i>V. lilacina</i> 'De La Mina'	Lilac Verbena	N, L	3' x 3'	
N = native; VL = very low; L = low; M = medium				



*Buchloe dactyloides UC Verde™*



*Bouteloua gracilis 'Hachita'*



*Carex pansa (C. praegracilis)*



*Achillea millifolium cultivar mowed*



Chamomile



*Dymondia margaretae*

**FIGURE 4-19A Example Species and Cultivars Suitable for Parkways and Setbacks**



*Carex divulsa*



*Festuca glauca*



*Sesleria autumnalis*



*Pennisetum 'Little Bunny'*



*Achillea 'Terra Cotta'*



*Aptenia cordifolia 'Red Apple'*



*Delosperma cooperi*



*Drosanthemum floribundum*



*Dudleya hassei*



*Erigeron glaucus 'Wayne Roderick'*



*Gazania rigens leucolaena*



*Gazania linearis*



*Fragaria chiloensis*



*Hypericum calycinum*



*Iris douglasiana*



*Lantana 'Patriot Rainbow'*

FIGURE 4-19B Example Species and Cultivars Suitable for Parkways and Setbacks



*Lessingia filaginifolia* 'Silver Carpet'



*Monardella villosa*



*Nepeta mussinii*



*Osteospermum fruitcosum*



*Oenothera caespitosa*



*Rosmarinus officinalis*



*Scaevola aemula*



*Senecio mandraeolisciae*



*Thymus*



*Verbena peruviana* varieties



*Vinca minor*



*Ceanothus* 'Centennial'



*Cotoneaster dammeri*



*Juniperus procumbens*



*Juniperus horizontalis* var.

FIGURE 4-19C Example Species and Cultivars Suitable for Parkways and Setbacks



FIGURE 4-19D Example Species and Cultivars Suitable for Parkways and Setbacks



*Ceanothus gloriosus* 'Anchor Bay'



*Ceanothus gloriosus* 'Anchor Bay'



*Cistus salvifolius*



*Iva hayesiana*



*Lantana montevidensis*



*Lantana* 'Gold Rush'



*Mimulus* 'Jelly Bean Yellow'



White Flower Carpet Rose



Red Flower Carpet Rose



Amber Flower Carpet Rose



*Salvia apiana*



*Salvia* 'Bee's Bliss'



*Verbena lilacina*

FIGURE 4-19E Example Species and Cultivars Suitable for Parkways and Setbacks